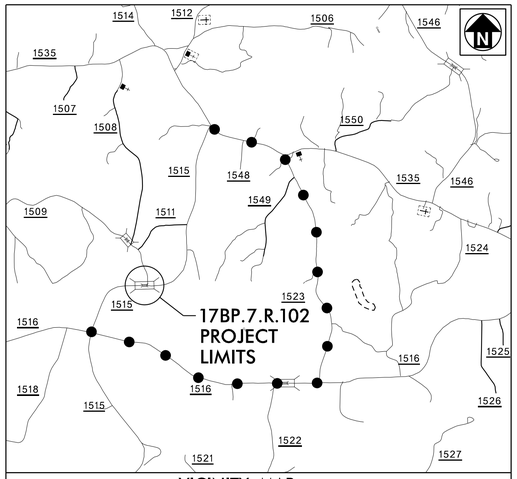
CONTRACT: TIP PROJECT: 17BP.7.R.102



VICINITY MAP
(NOT TO SCALE)

OFF–SITE DETOUR

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

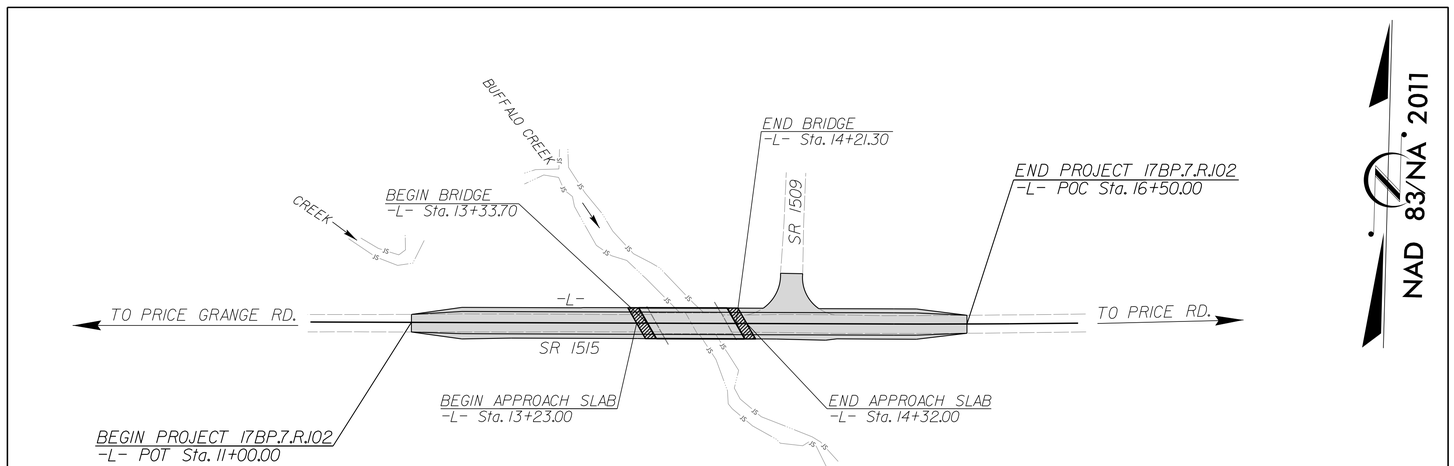
ROCKINGHAM COUNTY

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	17BP.7.R.102	1	

STATE PROJECT NO.	F.A. PROJ. NO.	DESCRIPTION

LOCATION: BRIDGE NO. 171 OVER BUFFALO CREEK ON SR 1515 (SNEAD ROAD)

TYPE OF WORK: GRADING, PAVING, DRAINAGE AND STRUCTURE



*DESIGN EXCEPTION:
SAG VERTICAL CURVE K
VERTICAL SSD

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION

DESIGN DATA

ADT 2013 = 770

ADT 2025 = 1540

V = 55 MPH

SUB REGIONAL TIER
LOCAL

PROJECT LENGTH

LENGTH ROADWAY TIP PROJECT = 0.087 MILES

LENGTH STRUCTURE TIP PROJECT = 0.017 MILES

TOTAL LENGTH TIP PROJECT = 0.104 MILES

Prepared in the Office of Hatch Mott MacDonald for

DIVISION 7

NORTH CAROLINA DEPARTMENT OF TRANSPORTATION

2012 STANDARD SPECIFICATIONS

LETTING DATE:

TIM JORDAN, PE
PROJECT ENGINEER

DAVID FUH, PE
HYDRAULICS ENGINEER

NCDOT CONTACT: TIM POWERS, PE
DIVISION BRIDGE PROGRAM MANAGER

ROADWAY DESIGN ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER TIMOTHY JORDAN

DocuSigned by: Jane Timothy Jordan 10/19/2016
SIGNATURE: *P.E.*

HYDRAULICS ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 19732 ENGINEER SHIH-CHEN FUH

DocuSigned by: Shih-Chen Fuh 10/19/2016
SIGNATURE: *P.E.*

PLANS PREPARED BY:

MOTT MACDONALD
PO Box 700
Fuquay–Varina, NC 27526
(919) 552–2253
(919) 552–2254 (Fax)
www.mottmac.com/americas

LICENSE NO. F–0669

ICA Engineering
5121 Kingdom Way, Suite 100
Raleigh, NC 27607
NC License No: P-0258

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PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	1-A

ROADWAY DESIGN ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER JASON T. WALKER 10/19/2016

MOTT MACDONALD I & E, LLC
LICENSE NO. F-0669

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

Prepared in the Office of:
MOTT MACDONALD
PO Box 700
Fuquay-Varina, NC 27526
www.mottmac.com/americas

GENERAL NOTES: 2012 SPECIFICATIONS
EFFECTIVE: 01-17-12
REVISED: 10-31-14

GRADE LINE:
GRADING AND SURFACING:

THE GRADE LINES SHOWN DENOTE THE FINISHED ELEVATION OF THE PROPOSED SURFACING AT GRADE POINTS SHOWN ON THE TYPICAL SECTIONS. GRADE LINES MAY BE ADJUSTED AT THEIR BEGINNING AND ENDING AND AT STRUCTURES AS DIRECTED BY THE ENGINEER IN ORDER TO SECURE A PROPER TIE-IN.

CLEARING:

CLEARING ON THIS PROJECT SHALL BE PERFORMED TO THE LIMITS ESTABLISHED BY METHOD II.

SUPERELEVATION:

ALL CURVES ON THIS PROJECT SHALL BE SUPERELEVATED IN ACCORDANCE WITH STD. NO. 225.04 USING THE RATE OF SUPERELEVATION AND RUNOFF SHOWN ON THE PLANS. SUPERELEVATION IS TO BE REVOLVED ABOUT THE GRADE POINTS SHOWN ON THE TYPICAL SECTIONS.

SHOULDER CONSTRUCTION:

ASPHALT, EARTH, AND CONCRETE SHOULDER CONSTRUCTION ON THE HIGH SIDE OF SUPERELEVATED CURVES SHALL BE IN ACCORDANCE WITH STD. NO. 560.01.

SIDE ROADS:

THE CONTRACTOR WILL BE REQUIRED TO DO ALL NECESSARY WORK TO PROVIDE SUITABLE CONNECTIONS WITH ALL ROADS, STREETS, AND DRIVES ENTERING THIS PROJECT. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PARTICULAR ITEMS INVOLVED.

GUARDRAIL:

THE GUARDRAIL LOCATIONS SHOWN ON THE PLANS MAY BE ADJUSTED DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHOULD CONSULT WITH THE ENGINEER PRIOR TO ORDERING GUARDRAIL MATERIAL.

SUBSURFACE PLANS:

NO SUBSURFACE PLANS ARE AVAILABLE ON THIS PROJECT. THE CONTRACTOR SHOULD MAKE HIS OWN INVESTIGATION AS TO THE SUBSURFACE CONDITIONS.

END BENTS:

THE ENGINEER SHALL CHECK THE STRUCTURE END BENT PLANS, DETAILS, AND CROSS-SECTION PRIOR TO SETTING OF THE SLOPE STAKES FOR THE EMBANKMENT OR EXCAVATION APPROACHING A BRIDGE.

UTILITIES:

UTILITY OWNER ON THIS PROJECT IS CENTURY LINK.

ANY RELOCATION OF EXISTING UTILITIES WILL BE ACCOMPLISHED BY OTHERS.

INDEX OF SHEETS

EFF: 01-17-12
REV: 02-29-2016

2012 ROADWAY ENGLISH STANDARD DRAWINGS

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N. C. Department of Transportation - Raleigh, N. C., Dated January, 2012 are applicable to this project and by reference hereby are considered a part of these plans:

STD.NO.	TITLE
DIVISION 2 - EARTHWORK	
200.02	Method of Clearing - Method II
225.02	Guide for Grading Subgrade - Secondary and Local
225.04	Method of Obtaining Superelevation - Two Lane Pavement
DIVISION 3 - PIPE CULVERTS	
300.01	Method of Pipe Installation
310.10	Driveway Pipe Construction
DIVISION 4 - MAJOR STRUCTURES	
422.11	Bridge Approach Fills - Sub Regional Tier
DIVISION 5 - SUBGRADE, BASES AND SHOULDERS	
560.01	Method of Shoulder Construction - High Side of Superelevated Curve - Method I
DIVISION 6 - ASPHALT BASES AND PAVEMENTS	
654.01	Pavement Repairs
DIVISION 8 - INCIDENTALS	
840.00	Concrete Base Pad for Drainage Structures
840.25	Anchorage for Frames - Brick or Concrete or Precast
840.29	Frames and Narrow Slot Flat Grates
840.35	Traffic Bearing Grated Drop Inlet - for Cast Iron Double Frame and Grates
840.46	Traffic Bearing Precast Drainage Structure
840.66	Drainage Structure Steps
840.72	Pipe Collar
846.01	Concrete Curb, Gutter and Curb & Gutter
846.04	Drop Inlet Installation in Shoulder Berm Gutter
862.01	Guardrail Placement
862.02	Guardrail Installation
876.01	Rip Rap in Channels
876.02	Guide for Rip Rap at Pipe Outlets
876.04	Drainage Ditches with Class 'B' Rip Rap

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PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	1-B

Note: Not to Scale

****S.U.E. = Subsurface Utility Engineering***

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

CONVENTIONAL PLAN SHEET SYMBOLS

BOUNDARIES AND PROPERTY:

- State Line
- County Line
- Township Line
- City Line
- Reservation Line
- Property Line
- Existing Iron Pin — EIP
- Property Corner
- Property Monument — ECM
- Parcel/Sequence Number — 123
- Existing Fence Line
- Proposed Woven Wire Fence
- Proposed Chain Link Fence
- Proposed Barbed Wire Fence
- Existing Wetland Boundary — WLB
- Proposed Wetland Boundary — WLB
- Existing Endangered Animal Boundary — EAB
- Existing Endangered Plant Boundary — EPB
- Known Soil Contamination: Area or Site
- Potential Soil Contamination: Area or Site

BUILDINGS AND OTHER CULTURE:

- Gas Pump Vent or U/G Tank Cap
- Sign — S
- Well — W
- Small Mine
- Foundation
- Area Outline
- Cemetery
- Building
- School
- Church
- Dam

HYDROLOGY:

- Stream or Body of Water
- Hydro, Pool or Reservoir
- Jurisdictional Stream — JS
- Buffer Zone 1 — BZ 1
- Buffer Zone 2 — BZ 2
- Flow Arrow
- Disappearing Stream
- Spring
- Wetland
- Proposed Lateral, Tail, Head Ditch — FLOW
- False Sump

RAILROADS:

- Standard Gauge — CSX TRANSPORTATION
- RR Signal Milepost — MILEPOST 35
- Switch — SWITCH
- RR Abandoned
- RR Dismantled

RIGHT OF WAY:

- Baseline Control Point
- Existing Right of Way Marker
- Existing Right of Way Line
- Proposed Right of Way Line — R/W
- Proposed Right of Way Line with Iron Pin and Cap Marker — R/W
- Proposed Right of Way Line with Concrete or Granite R/W Marker — R/W
- Proposed Control of Access Line with Concrete C/A Marker — C/A
- Existing Control of Access — C/A
- Proposed Control of Access — C/A
- Existing Easement Line — E
- Proposed Temporary Construction Easement — E
- Proposed Temporary Drainage Easement — TDE
- Proposed Permanent Drainage Easement — PDE
- Proposed Permanent Drainage / Utility Easement — DUE
- Proposed Permanent Utility Easement — PUE
- Proposed Temporary Utility Easement — TUE
- Proposed Aerial Utility Easement — AUE
- Proposed Permanent Easement with Iron Pin and Cap Marker

ROADS AND RELATED FEATURES:

- Existing Edge of Pavement
- Existing Curb
- Proposed Slope Stakes Cut — C
- Proposed Slope Stakes Fill — F
- Proposed Curb Ramp — CR
- Existing Metal Guardrail
- Proposed Guardrail
- Existing Cable Guiderail
- Proposed Cable Guiderail
- Equality Symbol
- Pavement Removal

VEGETATION:

- Single Tree
- Single Shrub
- Hedge
- Woods Line
- Orchard
- Vineyard — Vineyard

EXISTING STRUCTURES:

MAJOR:

- Bridge, Tunnel or Box Culvert — CONC
- Bridge Wing Wall, Head Wall and End Wall — CONC WW

MINOR:

- Head and End Wall — CONC HW
- Pipe Culvert
- Footbridge
- Drainage Box: Catch Basin, DI or JB — CB
- Paved Ditch Gutter
- Storm Sewer Manhole — S
- Storm Sewer — S

UTILITIES:

POWER:

- Existing Power Pole
- Proposed Power Pole
- Existing Joint Use Pole
- Proposed Joint Use Pole
- Power Manhole — P
- Power Line Tower
- Power Transformer
- U/G Power Cable Hand Hole
- H-Frame Pole
- Recorded U/G Power Line — P
- Designated U/G Power Line (S.U.E.*) — P

TELEPHONE:

- Existing Telephone Pole
- Proposed Telephone Pole
- Telephone Manhole — T
- Telephone Booth
- Telephone Pedestal — T
- Telephone Cell Tower
- U/G Telephone Cable Hand Hole — HH
- Recorded U/G Telephone Cable — T
- Designated U/G Telephone Cable (S.U.E.*) — T
- Recorded U/G Telephone Conduit — TC
- Designated U/G Telephone Conduit (S.U.E.*) — TC
- Recorded U/G Fiber Optics Cable — T FO
- Designated U/G Fiber Optics Cable (S.U.E.*) — T FO

WATER:

- Water Manhole — W
- Water Meter
- Water Valve
- Water Hydrant
- Recorded U/G Water Line — W
- Designated U/G Water Line (S.U.E.*) — W
- Above Ground Water Line — A/G Water

TV:

- TV Satellite Dish
- TV Pedestal — C
- TV Tower
- U/G TV Cable Hand Hole — HH
- Recorded U/G TV Cable — TV
- Designated U/G TV Cable (S.U.E.*) — TV
- Recorded U/G Fiber Optic Cable — TV FO
- Designated U/G Fiber Optic Cable (S.U.E.*) — TV FO

GAS:

- Gas Valve
- Gas Meter
- Recorded U/G Gas Line — G
- Designated U/G Gas Line (S.U.E.*) — G
- Above Ground Gas Line — A/G Gas

SANITARY SEWER:

- Sanitary Sewer Manhole
- Sanitary Sewer Cleanout
- U/G Sanitary Sewer Line — SS
- Above Ground Sanitary Sewer — A/G Sanitary Sewer
- Recorded SS Forced Main Line — FSS
- Designated SS Forced Main Line (S.U.E.*) — FSS

MISCELLANEOUS:

- Utility Pole
- Utility Pole with Base
- Utility Located Object
- Utility Traffic Signal Box — S
- Utility Unknown U/G Line — ?UTL
- U/G Tank; Water, Gas, Oil
- Underground Storage Tank, Approx. Loc. — UST
- A/G Tank; Water, Gas, Oil
- Geoenvironmental Boring
- U/G Test Hole (S.U.E.*)
- Abandoned According to Utility Records — AATUR
- End of Information — E.O.I.

PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	2

ROADWAY DESIGN ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER JAMES TIMOTHY JORDAN

DocuSigned by: James Timothy Jordan 3A6529B59D22444

10/19/2016

MOTT MACDONALD I & E, LLC
LICENSE NO. F-0669

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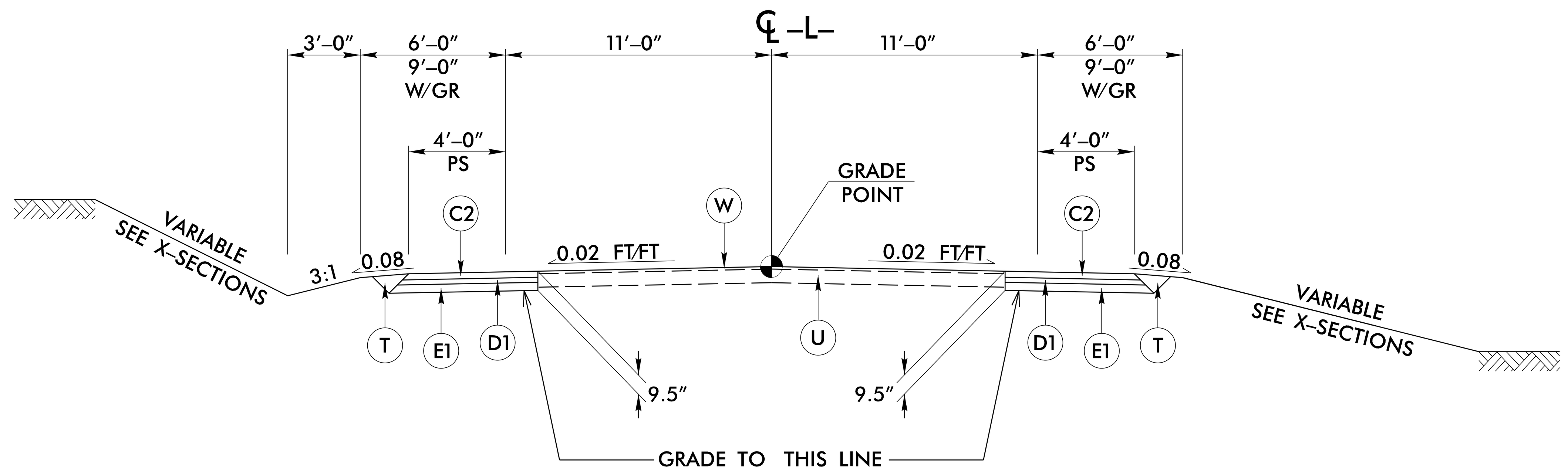
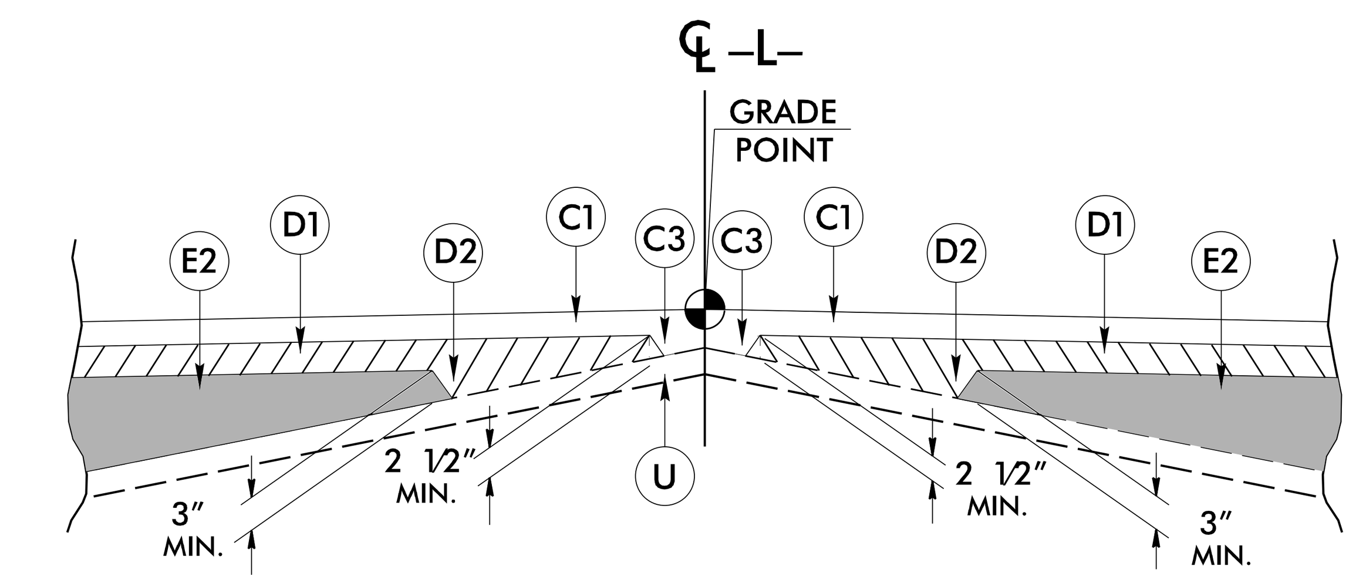


TYPICAL SECTION NO. 1

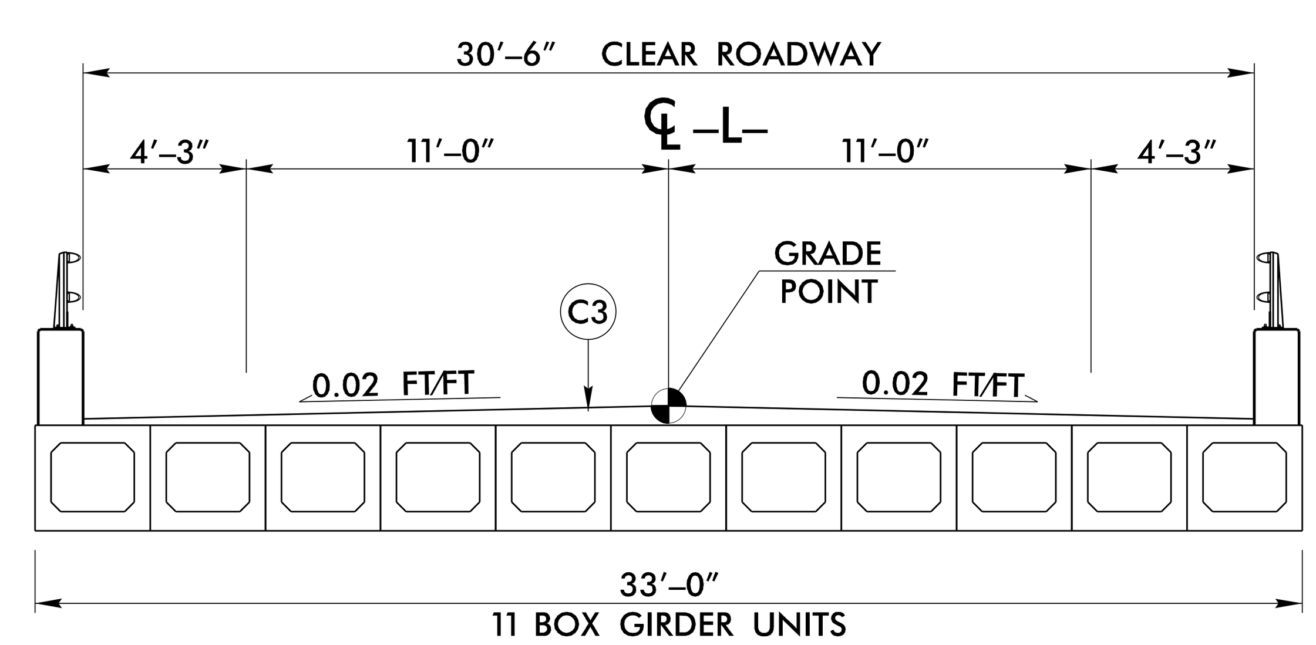
TRANSITION FROM EXISTING TO TYPICAL SECTION NO. 1:
–L– STA 11+00.00 TO 11+50.00

USE TYPICAL SECTION NO. 1:
–L– STA 11+50.00 TO 13+33.70 (BEGIN BRIDGE)
–L– STA 14+21.30 (END BRIDGE) TO 16+00.00

TRANSITION FROM TYPICAL SECTION NO. 1 TO EXISTING:
–L– STA 16+00.00 TO 16+50.00



Detail Showing Method of Wedging



TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2:
–L– STA 13+33.70 (BEGIN BRIDGE) TO 14+21.30 (END BRIDGE)

NOTE: SEE STRUCTURE PLANS FOR PAVEMENT DEPTHS ON STRUCTURE

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.
C3	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 1½" IN DEPTH OR GREATER THAN 2" IN DEPTH.
D1	PROP. APPROX. 2½" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
D2	PROP. VAR. DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH, TO BE PLACED IN LAYERS NOT LESS THAN 2½" IN DEPTH OR GREATER THAN 4" IN DEPTH.
E1	PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
E2	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5½" IN DEPTH.
T	EARTH MATERIAL.
U	EXISTING PAVEMENT.
W	WEDGING (SEE DETAIL SHOWING METHOD OF WEDGING).

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

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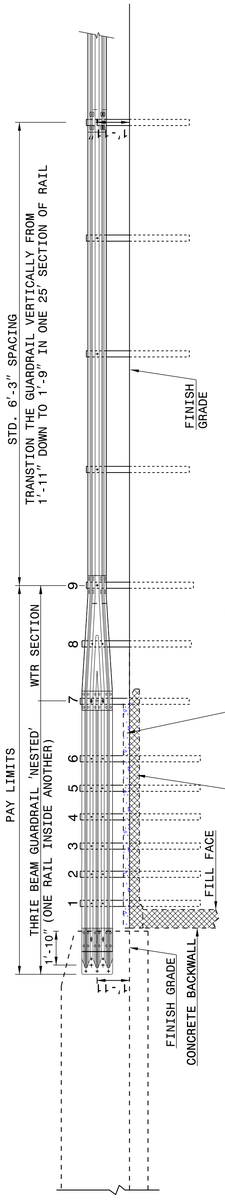
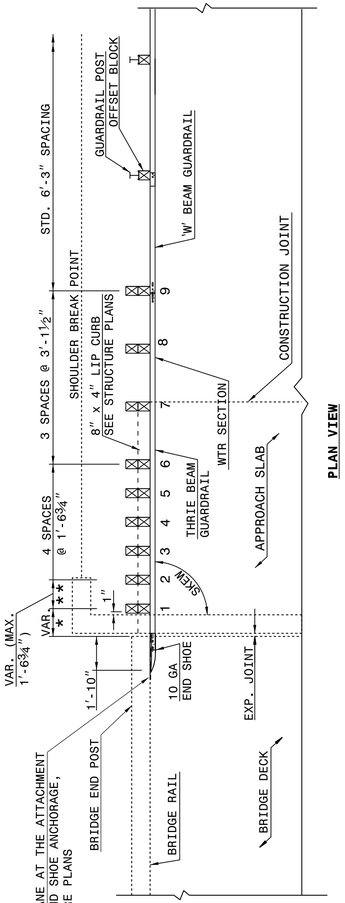
PROJECT REFERENCE NO.	SHEET NO.
17BP.7.R.102 - ROCKINGHAM 171	2-A



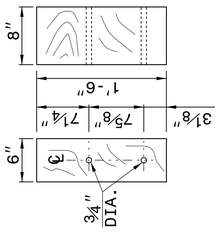
ELEVATION

NOTE:
**POST NOT REQUIRED FOR SKEW ANGLES GREATER THAN 150° OR LESS THAN 30° UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
*THE DISTANCE FROM END OF BRIDGE RAIL TO CENTER LINE OF THE FIRST POST SHOULD BE 1½" IF CONCRETE BACKWALL IS NOT PRESENT.
-SHOULDER BERM GUTTER MUST BE INSTALLED TO THE LIMITS 8" x 4" LIP CURB IS NOT ADJACENT TO AN APPROACH SLAB.
-MEASURE GUARDRAIL HEIGHT FROM THE TOP OF ADJACENT SURFACE (SHOULDER, BERM, OR GUTTER).
-LAP JOINTS IN THE DIRECTION OF TRAFFIC FLOW.
-SEE SHEET 5 FOR POST SECTIONS 1 THRU 9.

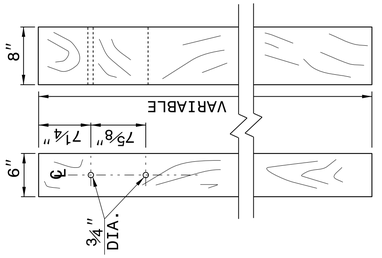


PLAN VIEW

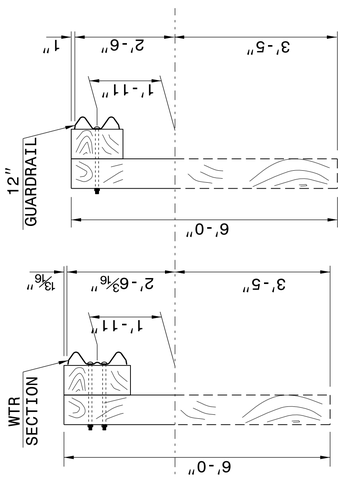
GUARDRAIL ANCHOR UNIT, TYPE III FOR ATTACHMENT TO RAIL ON BRIDGE - SUB REGIONAL TIER



THRIE BEAM OFFSET BLOCK

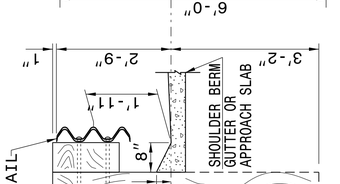


THRIE BEAM LINE POST

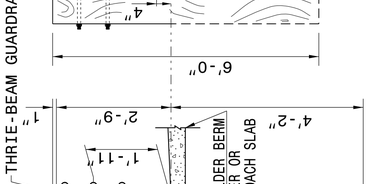


SECTION OF 'W' BEAM POST 9

SECTION OF WTR BEAM POST 8

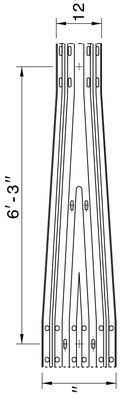


SECTION OF THRIE BEAM POST 7

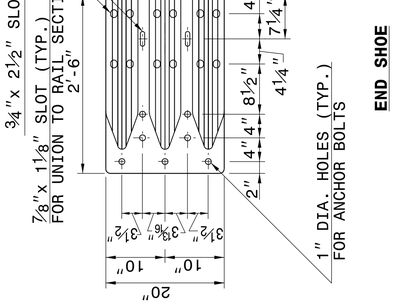


SECTION OF THRIE BEAM POSTS 1 THRU 6

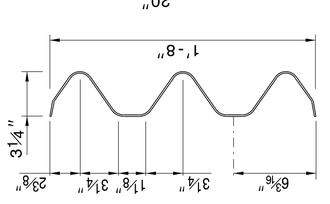
NOTE: THE MID POST AND OFFSET BLOCK OF THE WTR SECTION WILL REQUIRE SPECIAL BOLT HOLE DRILLING IN THE THRIE BEAM OFFSET BLOCK AND LINE POST.



WTR SECTION
ELEVATION VIEW



END SHOE



THRIE-BEAM SECTION

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

SEE TITLE BLOCK

ORIGINAL BY: J HOWERTON DATE: 06-22-12
MODIFIED BY: _____ DATE: _____
CHECKED BY: _____ DATE: _____
FILE SPEC.: _____

$$$$SYSTIME$$$$
$$$$USERNAME$$$$

"N" = DISTANCE FROM EDGE OF LANE TO FACE OF GUARDRAIL.
TOTAL SHOULDER WIDTH = DISTANCE FROM EDGE OF TRAVEL LANE TO SHOULDER BREAK POINT.
FLARE LENGTH = DISTANCE FROM LAST SECTION OF PARALLEL GUARDRAIL TO END OF GUARDRAIL.
W = TOTAL WIDTH OF FLARE FROM BEGINNING OF TAPER TO END OF GUARDRAIL.
G = GATING IMPACT ATTENUATOR TYPE 350
NG = NON–GATING IMPACT ATTENUATOR TYPE 350

GUARDRAIL SUMMARY

SURVEY LINE	BEG. STA.	END STA.	LOCATION	LENGTH			WARRANT POINT		"N" DIST. FROM E.O.L.	TOTAL SHOULDER WIDTH	FLARE LENGTH		W		ANCHORS									IMPACT ATTENUATOR TYPE 350			REMARKS
				STRAIGHT	SHOP CURVED	DOUBLE FACED	APPROACH END	TRAILING END			APPROACH END	TRAILING END	APPROACH END	TRAILING END	AT–1	GRAU 350	TYPE III							NO.	PERMITTED G	PERMITTED NG	
–L–	11+48.75	13+42.50	RT	193.75′			13+42.50 (BRIDGE)		6′	9′						1	1										
–L–	11+94.15	13+24.90	LT	131.25′				13+24.90 (BRIDGE)	6′	9′						1	1										
–L–	14+30.10	15+11.35	RT	81.25′				14+30.10 (BRIDGE)	6′	9′						1	1										
–L–	14+12.50	14+56 +/–	LT	25.00′	37.50′		14+12.50 (BRIDGE)		6′	9′					1		1										BREAK FOR DRIVE
		SUBTOTAL		431.25′	37.50′																						
		LESS ANCHOR DEDUCTIONS																									
		GRAU–350 3 x 50.00′ =		–150.00′																							
		TYPE III 4 x 18.75′ =		–75.00′																							
		AT–1 1 x 6.25′ =		–6.25′																							
		TOTAL		200.00′	37.50′										1	3	4										

SUB–REGIONAL & REGIONAL
LIST OF PIPES, ENDWALLS, ETC. (FOR PIPES 48" & UNDER)

STATION (SIZE / THICKNESS OR GAUGE)	LOCATION (LT,RT, OR CL)	STRUCTURE NO. FROM	STRUCTURE NO. TO	TOP ELEVATION	INVERT ELEVATION	INVERT ELEVATION	SLOPE CRITICAL	DRAINAGE PIPE (RCP, CSP, CAAP, HDPE, or PVC) 12"	15"	18"	24"	30"	36"	42"	48"	DO NOT USE RCP	DO NOT USE PVC	DO NOT USE CAAP	DO NOT USE HDPE
13+01 +/–	LT	401	402	680.9	677.6	677.5	X												
13+01 +/–	RT	402	403	680.9	677.5	676.1			20′										
14+40 +/–	LT	404	405	676.9	677.0														
15+05 +/–	LT	406	407	677.8	679.3														
11+63 +/–	LT	408			676.9	675.7													
TOTAL									20′										

STATION	C.S. PIPE 12" (.064)	15" (.064)	18" (.064)	24" (.064)	36" (.079)	42" (.109)	48" (.109)	R.C. PIPE (CLASS III) 15"	18"	24"	30"	36"	42"	48"	R.C. PIPE (CLASS IV) 12"	15"	18"	24"	30"	36"	42"	48"
13+01 +/–																28′						
13+01 +/–																						
14+40 +/–										12′												
15+05 +/–										12′												
11+63 +/–																						
TOTAL										24′						28′						

STATION	**" R. C. PIPE (CLASS V)	**" R. C. PIPE CULVERTS, CONTRACTOR DESIGN PIPE	**" R. C. PIPE CULVERTS, CONTRACTOR DESIGN PIPE	15" SIDE DRAIN PIPE	18" SIDE DRAIN PIPE	ENDWALLS STD. 838.01, STD. 838.11 OR STD. 838.80 (UNLESS NOTED OTHERWISE) CU. YDS. R.C.P.	C.S.P.	QUANTITIES FOR DRAINAGE STRUCTURES PER EACH (0' THRU 5.0')	* TOTAL L.F. FOR PAY QUANTITY SHALL BE COL. 'A' + (1.3 X COL.'B') LIN. *FT. A 5.0' THRU 10.0'	B 10.0' AND ABOVE	C.B. STD. 840.01 OR STD. 840.02	FRAME, GRATES AND HOOD STANDARD 840.03 TYPE OF GRATE E	F	G		CONCRETE TRANSITIONAL SECTION CATCH BASIN	DROP INLET	G.D.I. FRAME WITH GRATE STD. 840.22	G.D.I. FRAME WITH TWO GRATES STD. 840.22	G.D.I. (N.S.) FRAME WITH GRATE STD. 840.24	G.D.I. (N.S.) FRAME WITH TWO GRATES STD. 840.24	G.D.I. (N.S.) FRAME WITH TWO GRATES STD. 840.29	T.B.D.I. STD. 840.35	CORR. STEEL ELBOWS NO. & SIZE	CONC. COLLARS CL. "B" C.Y. STD 840.72	CONC. & BRICK PIPE PLUG, C.Y. STD. 840.71	PIPE REMOVAL LIN.FT.	REMARKS
13+01 +/–								1														1	1					
13+01 +/–								1														1	1					
14+40 +/–																									0.60			
15+05 +/–																									0.60			
11+63 +/–				52′																								
TOTAL				52′				2														2	2		1.20			

ABBREVIATIONS

C.B.	CATCH BASIN
N.D.I.	NARROW DROP INLET
D.I.	DROP INLET
G.D.I.	GRATED DROP INLET
G.D.I. (N.S.)	GRATED DROP INLET (NARROW SLOT)
J.B.	JUNCTION BOX
M.H.	MANHOLE
T.B.D.I.	TRAFFIC BEARING DROP INLET
T.B.J.B.	TRAFFIC BEARING JUNCTION BOX

NOTE: Invert Elevations are for Bid Purposes only and shall not be used for project construction stakeout.
See "Standard Specifications For Roads and Structures, Section 300–5".

SUMMARY OF EARTHWORK
IN CUBIC YARDS

LOCATION	UNCLASSIFIED EXCAVATION	UNDERCUT	EMBT+%	BORROW	WASTE
–L– 11+00.00 TO 13+33.70 (BEGIN BRIDGE)	22		392	370	
–L– 14+21.30 (END BRIDGE) TO 16+50.00	22		95	73	
SUBTOTAL	44		487	443	
WASTE IN LIEU OF BORROW					
PROJECT TOTAL	44			443	
5% TO REPLACE BORROW				23	
GRAND TOTAL	44			466	
SAY	50			490	

NOTE: Approximate quantities only. Unclassified Excavation, Borrow Excavation, Fine Grading, Clearing and Grubbing and Removal of Existing Asphalt Pavement will be paid for at the contract Lump Sum price for "Grading".

*DESIGN EXCEPTION:
SAG VERTICAL CURVE K
VERTICAL SSD

DETAIL A
TOE PROTECTION
(Not to Scale)

d = 1 Ft.

Type of Liner = CLASS B Rip-Rap

-L- STA. 11+50 TO 12+30 RT
-L- STA. 11+30 TO 13+14 LT
-L- STA. 14+23 TO 14+45 LT
-L- STA. 14+95 TO 16+10 LT

PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	4

ROADWAY DESIGN ENGINEER
NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER JAMES TIMOTHY TIMOTHY
Signed by:
11/14/2016

HYDRAULICS ENGINEER
NORTH CAROLINA PROFESSIONAL SEAL 19732 ENGINEER SUSAN CHEN FITH
Signed by:
11/14/2016

MOTT MACDONALD I & E, LLC
LICENSE NO. F-0669

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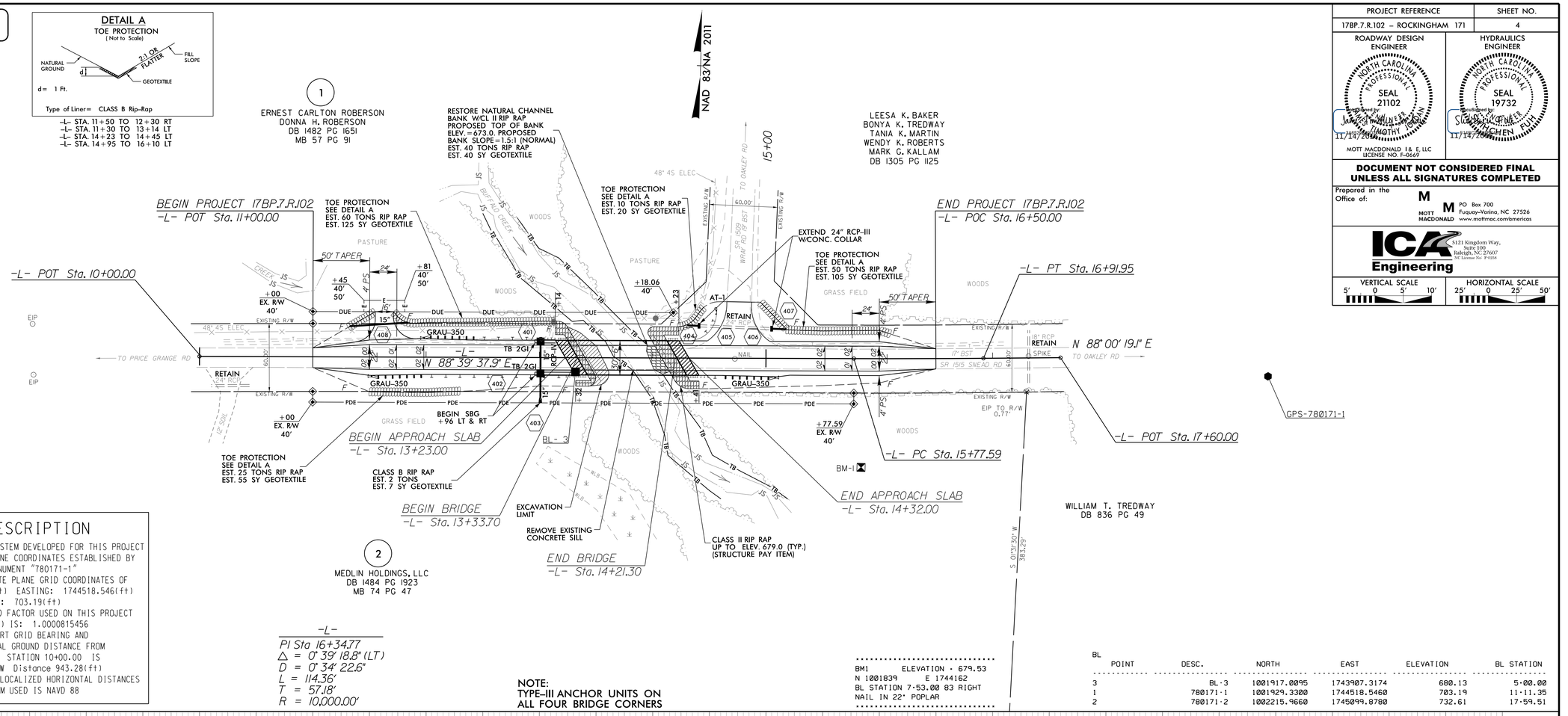
Prepared in the Office of:
MOTT MACDONALD
PO Box 700
Fuquay-Varina, NC 27526
www.mottmac.com/americas

ICA Engineering
5121 Kingdom Way, Suite 100
Raleigh, NC 27607
NC License No: P1025B

VERTICAL SCALE 5' 0 5' 10'
HORIZONTAL SCALE 25' 0 25' 50'



DATUM DESCRIPTION

THE LOCALIZED COORDINATE SYSTEM DEVELOPED FOR THIS PROJECT IS BASED ON THE STATE PLANE COORDINATES ESTABLISHED BY NCDOT FOR MONUMENT "780171-1"
WITH NAD 83/NA 2011 STATE PLANE GRID COORDINATES OF
NORTHING: 1001929.330(ft) EASTING: 1744518.546(ft)
ELEVATION: 703.19(ft)
THE AVERAGE COMBINED GRID FACTOR USED ON THIS PROJECT (GROUND TO GRID) IS: 1.0000815456
THE N.C. LAMBERT GRID BEARING AND LOCALIZED HORIZONTAL GROUND DISTANCE FROM "780171-1" TO -L- STATION 10+00.00 IS S 89° 32' 11.0" W Distance 943.28(ft)
ALL LINEAR DIMENSIONS ARE LOCALIZED HORIZONTAL DISTANCES
VERTICAL DATUM USED IS NAVD 88

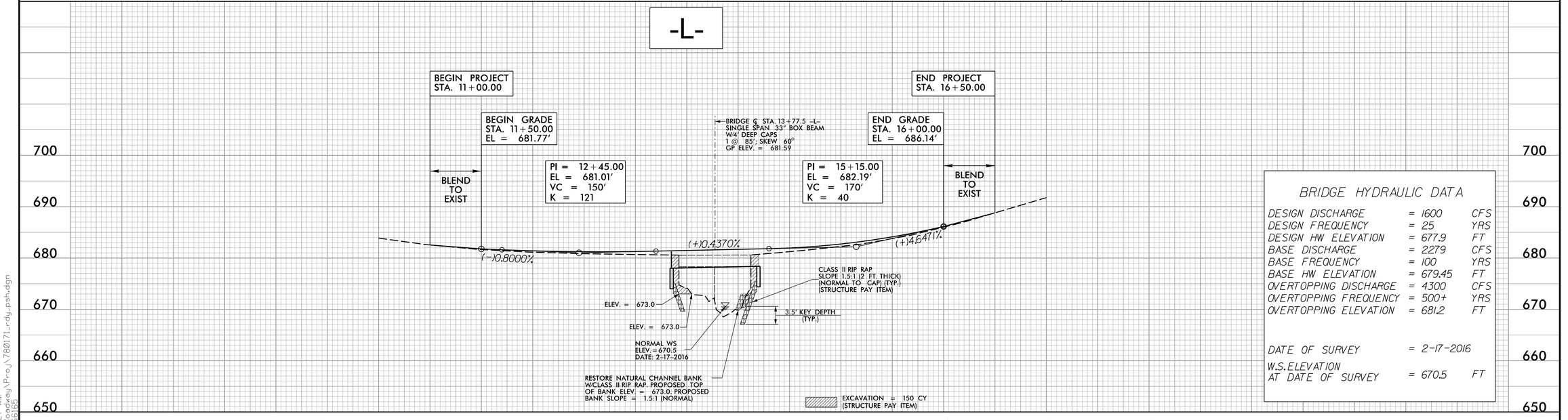
-L-
PI Sta 16+34.77
△ = 0° 39' 18.8" (LT)
D = 0° 34' 22.6"
L = 114.36'
T = 57.18'
R = 10,000.00'

NOTE:
TYPE-III ANCHOR UNITS ON ALL FOUR BRIDGE CORNERS

BM1 ELEVATION = 679.53
N 1001839 E 1744162
BL STATION 7+53.00 83 RIGHT
NAIL IN 22" POPLAR

BL

POINT	DESC.	NORTH	EAST	ELEVATION	BL STATION
3	BL-3	1001917.0095	1743907.3174	680.13	5+00.00
1	780171-1	1001929.3300	1744518.5460	703.19	11+11.35
2	780171-2	1002215.9660	1745099.8780	732.61	17+59.51



BRIDGE HYDRAULIC DATA

DESIGN DISCHARGE	= 1600	CFS
DESIGN FREQUENCY	= 25	YRS
DESIGN HW ELEVATION	= 677.9	FT
BASE DISCHARGE	= 2279	CFS
BASE FREQUENCY	= 100	YRS
BASE HW ELEVATION	= 679.45	FT
OVERTOPPING DISCHARGE	= 4300	CFS
OVERTOPPING FREQUENCY	= 500+	YRS
OVERTOPPING ELEVATION	= 681.2	FT
DATE OF SURVEY	= 2-17-2016	
W.S.ELEVATION AT DATE OF SURVEY	= 670.5	FT

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(or66165)

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" – HIGHWAY DESIGN BRANCH– N.C. DEPARTMENT OF TRANSPORTATION – RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD.	TITLE
1101.03	TEMPORARY ROAD CLOSURES
1110.01	STATIONARY WORK ZONE SIGNS
1145.01	BARRICADES
1205.01	PAVEMENT MARKINGS – LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS – TWO–LANE AND MULTI–LANE ROADWAYS
1205.12	PAVEMENT MARKINGS – BRIDGES
1261.01	GUARDRAIL AND BARRIER DELINEATORS – INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS – TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TRAFFIC PATTERN ALTERATIONS

A) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

B) PROVIDE PERMANENT SIGNING.

C) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING REQUIRED FOR THE OFF–SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

D) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.

COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF–SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.

E) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

F) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11–2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

PAVEMENT MARKINGS AND MARKERS

G) INSTALL PAVEMENT MARKINGS ON THE FINAL SURFACE.

jor66165
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PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	TMP–1

ROADWAY DESIGN ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER JAMES TIMOTHY JOHNSON

Digitally signed by JAMES TIMOTHY JOHNSON
10/19/2016

MOTT MACDONALD I & E, LLC
LICENSE NO. F–0669

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

Prepared in the Office of:
MOTT MACDONALD
PO Box 700
Fuquay–Varina, NC 27526
www.mottmac.com/americas

PHASING

STEP 1: USING ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 1 OF 9, AND SHEET TMP–2, PERFORM THE FOLLOWING:
– INSTALL ALL ROAD CLOSURE AND DETOUR SIGNING INCLUDING BARRICADES
– CLOSE SR 1515 (SNEAD ROAD)
– PLACE TRAFFIC ONTO OFF– SITE DETOUR

STEP 2: REMOVE EXISTING BRIDGE #171 AND CONSTRUCT THE PROPOSED BRIDGE AND APPROACHES AS SHOWN IN THE CONSTRUCTION PLANS.

STEP 3: INSTALL FINAL PAVEMENT MARKINGS.

STEP 4: REMOVE ALL TRAFFIC CONTROL SIGNING AND DEVICES AND RE–OPEN SR 1515 (SNEAD ROAD) TO THE FINAL TRAFFIC PATTERN.

PAVEMENT MARKING

2FT. – 6FT. WHITE MINISKIP (4")	36 LF
PAINT WHITE EDGELINE (4")	2,254 LF
PAINT YELLOW DOUBLE CENTER (4")	2,200 LF

NOTE: QUANTITY INCLUDES 2 APPLICATIONS OF EACH

PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	TMP–2

ROADWAY DESIGN ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 21102 ENGINEER JAMES TIMOTHY JOHNSON

Digitally signed by JAMES TIMOTHY JOHNSON
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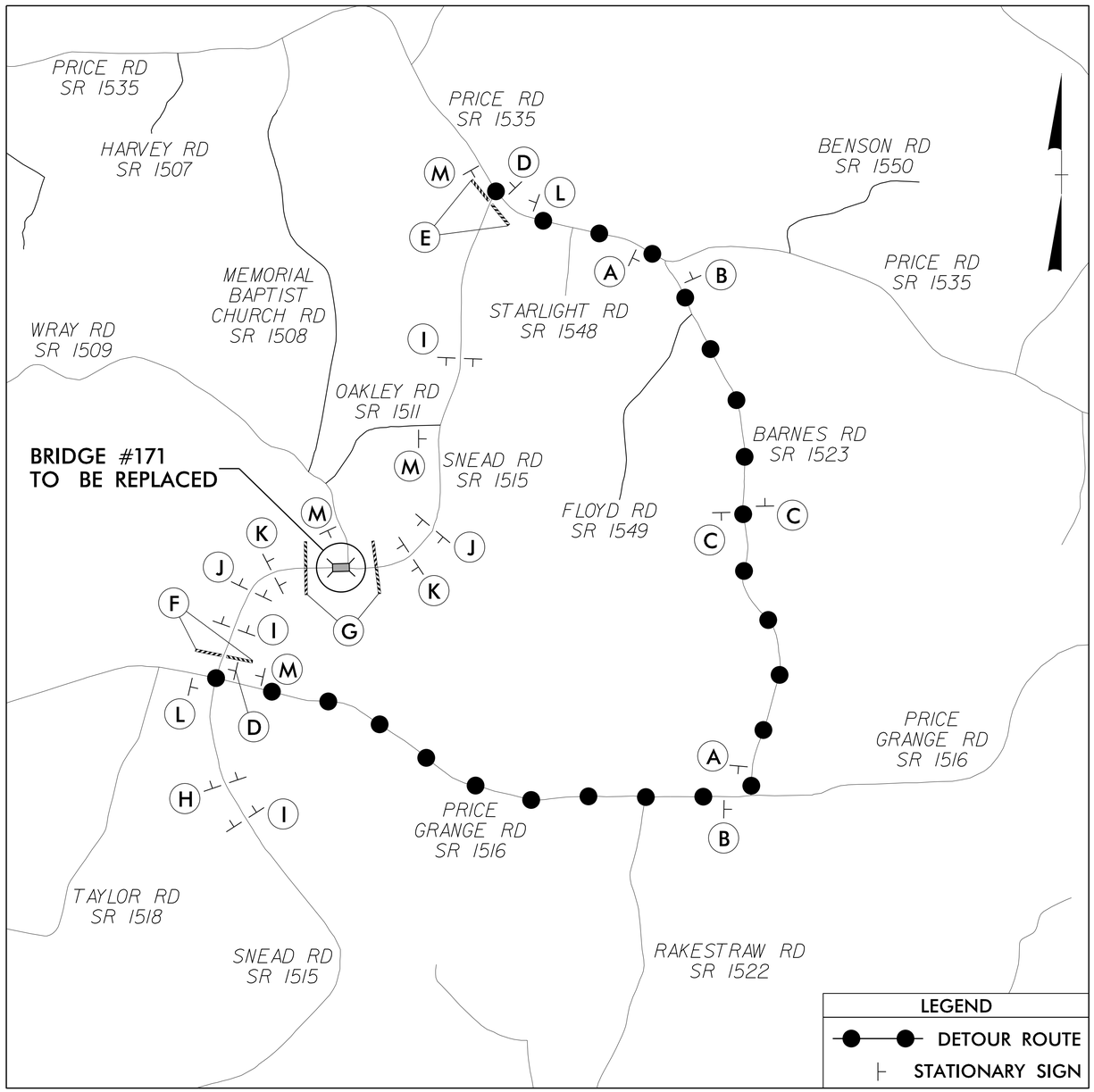

✱ SEE SHEET TMP–3 FOR SPECIAL SIGN DESIGNS

✱ SNEAD ROAD
DETOUR
M4–8 24″ X 12″
M6–1 R 21″ X 15″
A

✱ SNEAD ROAD
DETOUR
M4–8 24″ X 12″
M6–1 L 21″ X 15″
B

✱ SNEAD ROAD
DETOUR
M4–8 24″ X 12″
M6–3 21″ X 15″
C

END DETOUR
M4–8 A 24″ X 18″
D

R11-4 60″ x 30″
ROAD CLOSED TO THRU TRAFFIC
DETOUR
M4-10L 48″ x 18″
TYPE III BARRICADE
E

R11-4 60″ x 30″
ROAD CLOSED TO THRU TRAFFIC
DETOUR
M4-10R 48″ x 18″
TYPE III BARRICADE
F

R11-2 48″ x 30″
ROAD CLOSED
TYPE III BARRICADE(S)
G

DETOUR AHEAD
W20–2 48″ X 48″
H

ROAD CLOSED AHEAD
W20–3 48″ X 48″
I

ROAD CLOSED 1000 FT
W20–3 48″ X 48″
J

ROAD CLOSED 500 FT
W20–3 48″ X 48″
K

W20–3 48″ X 48″
ROAD CLOSED AHEAD
NEXT LEFT
SP–4L 42″ X 12″
L

W20–3 48″ X 48″
ROAD CLOSED AHEAD
NEXT RIGHT
SP–4R 42″ X 12″
M

jor66165
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PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	TMP–3

TRAFFIC ENGINEER

NORTH CAROLINA PROFESSIONAL SEAL 023488 ENGINEER NATHAN K. PHILLIPS

Docusigned by: Nathan K. Phillips 10/28/2015

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LICENSE NO. F-0669

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SIGN NUMBER: SD-1
TYPE: D
QUANTITY: SEE PLANS

SIGN WIDTH: 3′-0″
HEIGHT: 2′-6″
TOTAL AREA: 7.5 Sq.Ft.

BORDER TYPE: INSET
RECESS: 0.38″
WIDTH: 0.5″
RADII: 1.5″

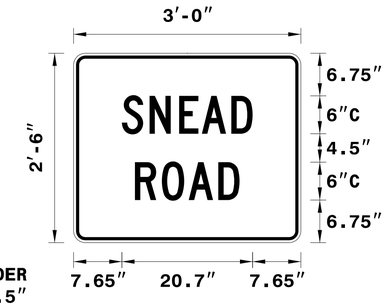
NO. Z BARS:
LENGTH:

BACKG COLOR: Fluorescent Orange
COPY COLOR: Black

SYMBOL	X	Y	WID	HT

MAT′L: 0.063″ (2.0 mm) ALUMINUM

DESIGN BY: PJ
PROJECT ID: 17BP.7.R.102
CHECKED BY: NKP
DIV: 7
DATE: Oct 20, 2015



BORDER
R=1.5″
TH=0.5″
IN=0.38″

Spacing Factor is 1 unless specified otherwise

USE NOTES: 1,2

1. Legend and border shall be direct applied black non-reflective sheeting.
2. Background shall be NC GRADE B fluoresent orange retroreflective sheeting.

LETTER POSITIONS

Letter locations are panel edge to lower left corner

																											Series/Size Text Length
	S	N	E	A	D																						C 2000
	7.7	12	16.7	20.3	25																						20.7
	R	O	A	D																							C 2000
	9.7	14	18.2	22.9																							16.6

FILENAME: 780171_rdy_tmp3

NORTH CAROLINA D.O.T. SIGN DETAIL

TIP PROJECT: 17BP.7.R.102

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	17BP.7.R.102	EC-1	

STATE PROJ. NO.	F. A. PROJ. NO.	DESCRIPTION

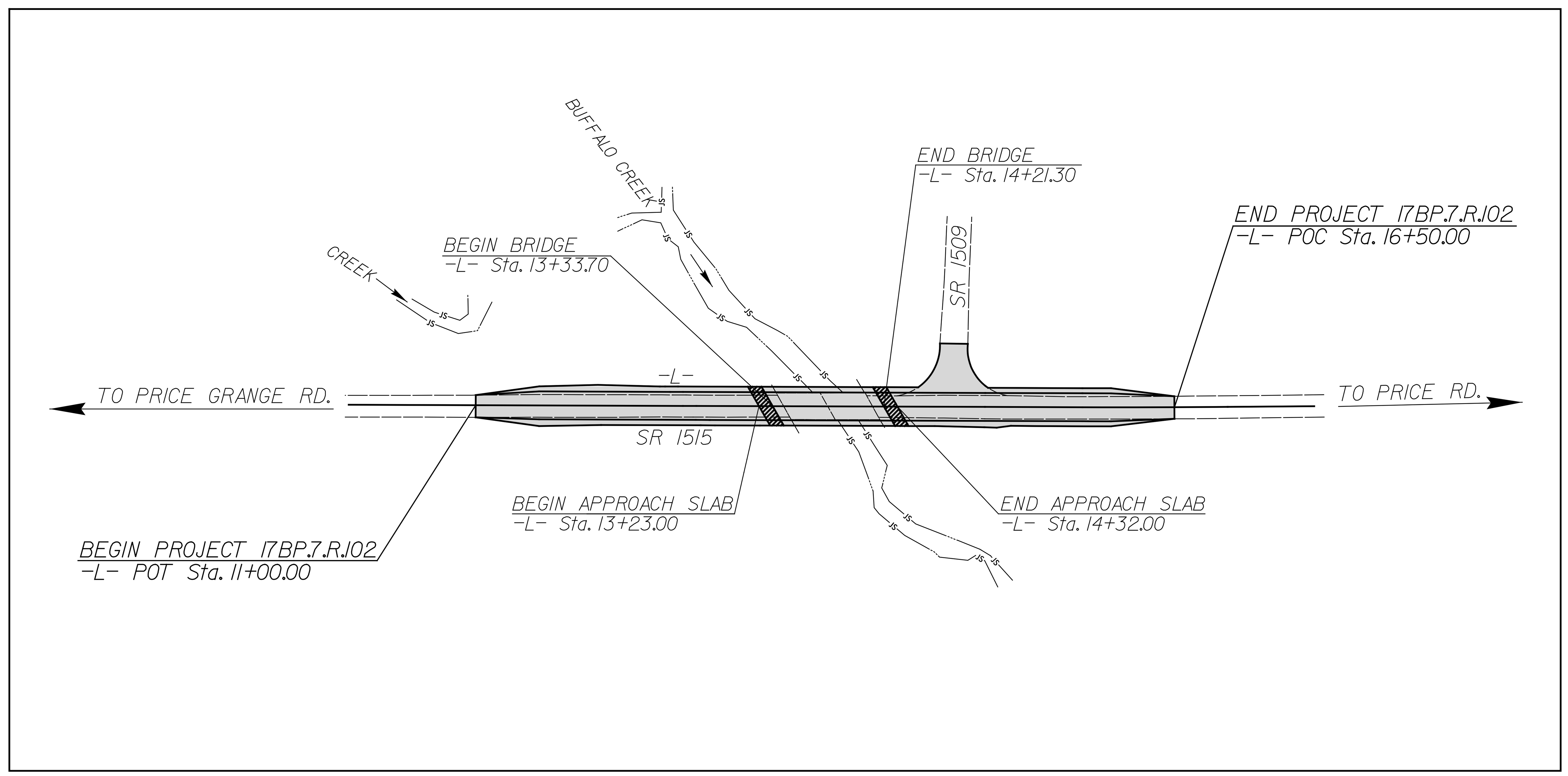
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

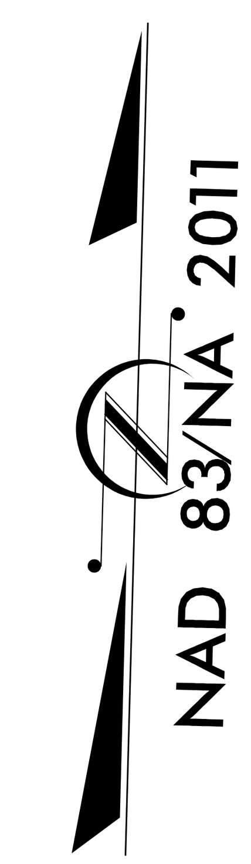
PLAN FOR PROPOSED
HIGHWAY EROSION CONTROL

ROCKINGHAM COUNTY

LOCATION: BRIDGE NO. 171 OVER BUFFALO CREEK ON SR 1515 (SNEAD ROAD)

TYPE OF WORK: GRADING, PAVING, DRAINAGE AND STRUCTURE





EROSION AND SEDIMENT CONTROL MEASURES

Std. #	Description	Symbol
1630.03	Temporary Silt Ditch	TSD
1630.05	Temporary Diversion	TD
1605.01	Temporary Silt Fence	
1606.01	Special Sediment Control Fence	
1622.01	Temporary Berms and Slope Drains	
1630.02	Silt Basin Type B	
1633.01	Temporary Rock Silt Check Type-A	
	Temporary Rock Silt Check Type-A with Matting and Polyacrylamide (PAM)	
1633.02	Temporary Rock Silt Check Type-B	
	Wattle/ Coir Fiber Wattle	EW / CFW
	Wattle/ Coir Fiber Wattle with Polyacrylamide (PAM)	
1634.01	Temporary Rock Sediment Dam Type-A	
1634.02	Temporary Rock Sediment Dam Type-B	
1635.01	Rock Pipe Inlet Sediment Trap Type-A	
1635.02	Rock Pipe Inlet Sediment Trap Type-B	
1630.04	Stilling Basin	
1630.06	Special Stilling Basin	
	Rock Inlet Sediment Trap:	
1632.01	Type A	A
1632.02	Type B	B
1632.03	Type C	C
	Skimmer Basin	
	Tiered Skimmer Basin	
	Infiltration Basin	

KYLE STOFFER, E.I.
ROADSIDE ENVIRONMENTAL ENGINEER

3844
LEVEL III CERTIFICATION NUMBER

STACEY H BAILEY, P.E.
ROADSIDE ENVIRONMENTAL PROJECT ENGINEER

3074
LEVEL III CERTIFICATION NUMBER

THIS PROJECT CONTAINS EROSION CONTROL PLANS FOR CLEARING AND GRUBBING PHASE OF CONSTRUCTION.

GRAPHIC SCALE

50
PLANS

50
PROFILE (HORIZONTAL)

10
PROFILE (VERTICAL)

THESE EROSION AND SEDIMENT CONTROL PLANS COMPLY WITH THE REGULATIONS SET FORTH BY THE NCG-010000 GENERAL CONSTRUCTION PERMIT EFFECTIVE AUGUST 1, 2016 AND ISSUED BY THE NORTH CAROLINA DEPARTMENT OF ENVIRONMENT AND NATURAL RESOURCES DIVISION OF WATER RESOURCES.

Prepared In the Office of:

ICA ENGINEERING
5121 KINGDOM WAY, SUITE 100
RALEIGH NC 27607
NC License No. F-0258

Designed by:

STACEY H. BAILEY, PE ***3074***
NAME *LEVEL III CERTIFICATION NO.*

Reviewed In the Office of:

ROADSIDE ENVIRONMENTAL UNIT
1 South Wilmington St.
Raleigh, NC 27611

2012 STANDARD SPECIFICATIONS

Reviewed by:

JEFF WALSTON, PE, CPESC, CPSWQ

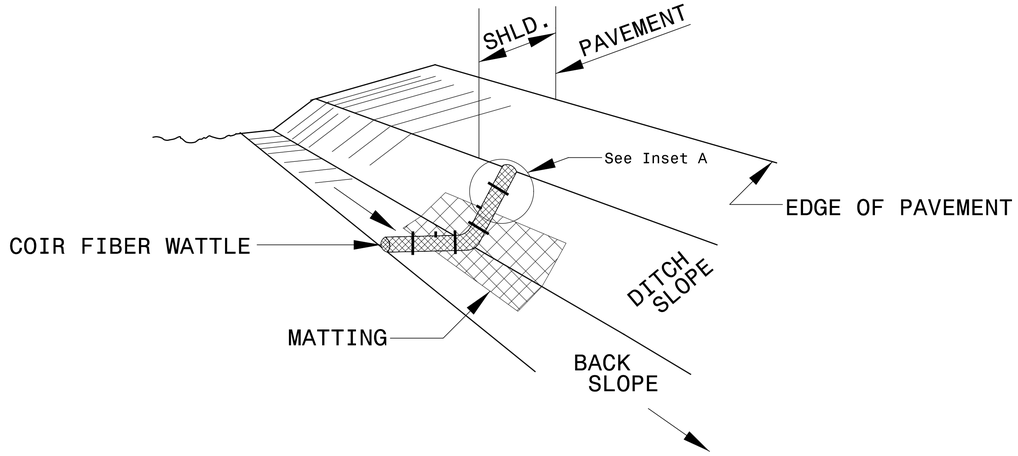
Roadway Standard Drawings

The following roadway english standards as appear in "Roadway Standard Drawings"- Roadway Design Unit - N. C. Department of Transportation - Raleigh, N. C., dated January 2012 and the latest revison thereto are applicable to this project and by reference hereby are considered a part of these plans.

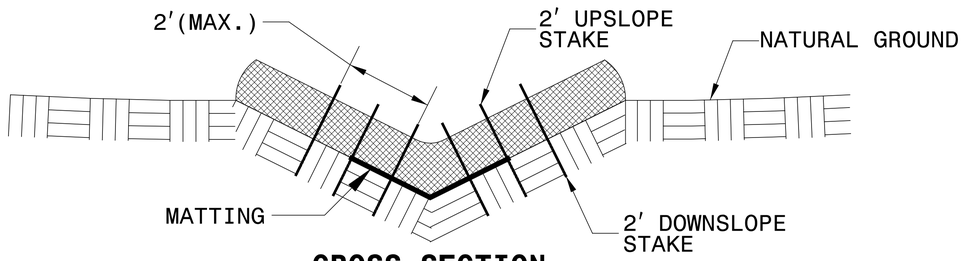
1604.01	Railroad Erosion Control Detail
1605.01	Temporary Silt Fence
1606.01	Special Sediment Control Fence
1607.01	Gravel Construction Entrance
1622.01	Temporary Berms and Slope Drains
1630.01	Riser Basin
1630.02	Silt Basin Type B
1630.03	Temporary Silt Ditch
1630.04	Stilling Basin
1630.05	Temporary Diversion
1630.06	Special Stilling Basin
1631.01	Matting Installation
1632.01	Rock Inlet Sediment Trap Type A
1632.02	Rock Inlet Sediment Trap Type B
1632.03	Rock Inlet Sediment Trap Type C
1633.01	Temporary Rock Silt Check Type A
1633.02	Temporary Rock Silt Check Type B
1634.01	Temporary Rock Sediment Dam Type A
1634.02	Temporary Rock Sediment Dam Type B
1635.01	Rock Pipe Inlet Sediment Trap Type A
1635.02	Rock Pipe Inlet Sediment Trap Type B
1640.01	Coir Fiber Baffle
1645.01	Temporary Stream Crossing

PROJECT REFERENCE NO.	SHEET NO.
17BP.7.RJ02	EC-2

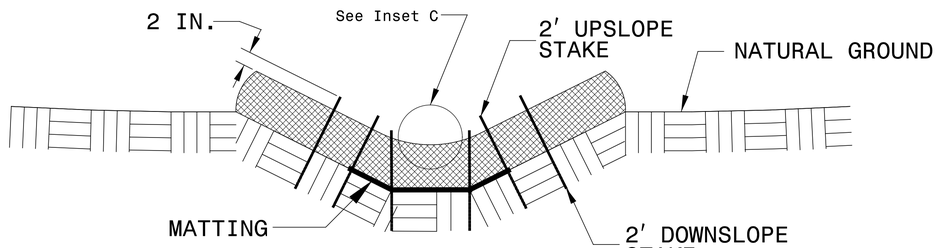
COIR FIBER WATTLE WITH POLYACRYLAMIDE (PAM) DETAIL



ISOMETRIC VIEW



CROSS SECTION VEE DITCH



CROSS SECTION TRAPEZOIDAL DITCH

NOTES:

USE MINIMUM 12 IN. DIAMETER COIR FIBER (COCONUT FIBER) WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

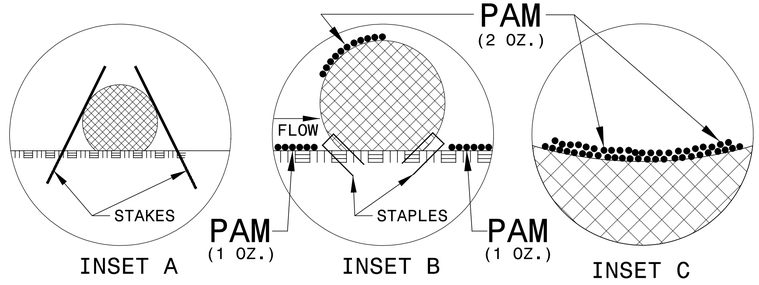
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12″ IN LENGTH.

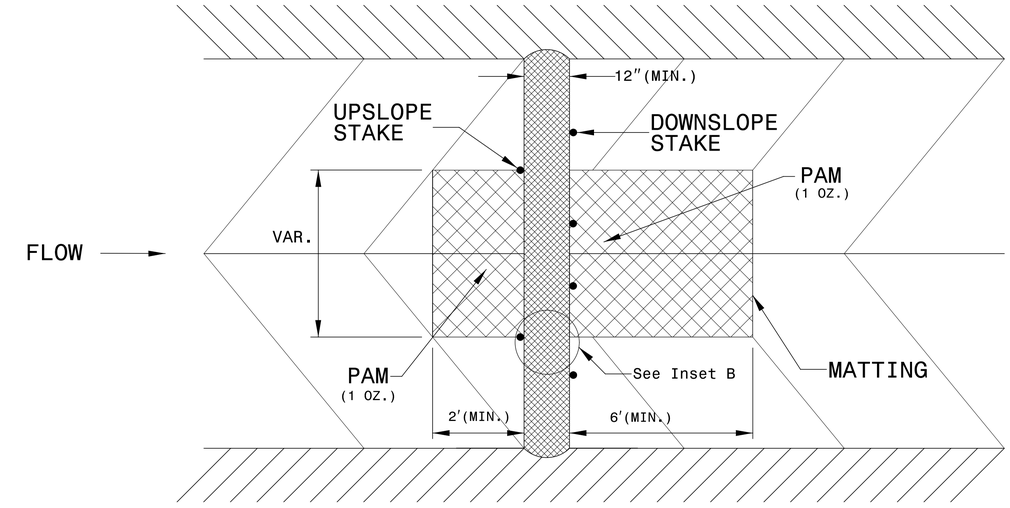
INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.

INITIALLY APPLY 2 OUNCES OF ANIONIC OR NEUTRALLY CHARGED PAM OVER WATTLE WHERE WATER WILL FLOW AND 1 OUNCE OF PAM ON MATTING ON EACH SIDE OF WATTLE. REAPPLY PAM AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.

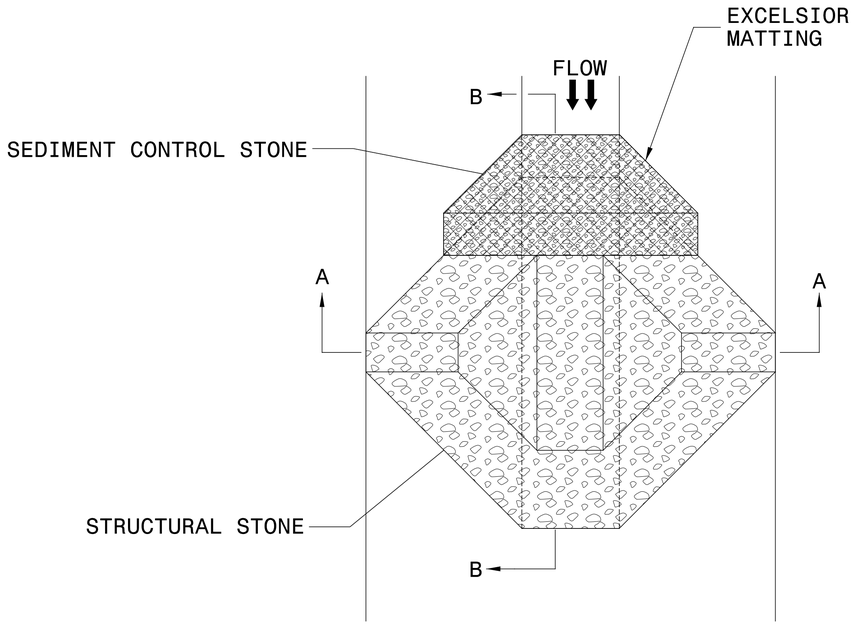




TOP VIEW

PROJECT REFERENCE NO.	SHEET NO.
17BP.7.RJ02	EC-2A

TEMPORARY ROCK SILT CHECK TYPE 'A' WITH EXCELSIOR MATTING AND POLYACRYLAMIDE (PAM)



PLAN

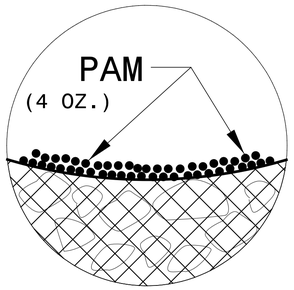
NOTES:

INSTALL TEMPORARY ROCK SILT CHECK TYPE A IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1633.01.

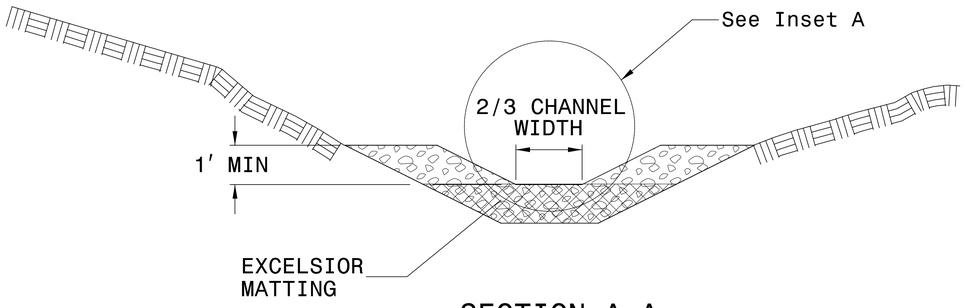
USE EXCELSIOR FOR MATTING MATERIAL AND ANCHOR MATTING SECTION AT TOP AND BOTTOM WITH CLASS B STONE.

PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH ROCK SILT CHECK.

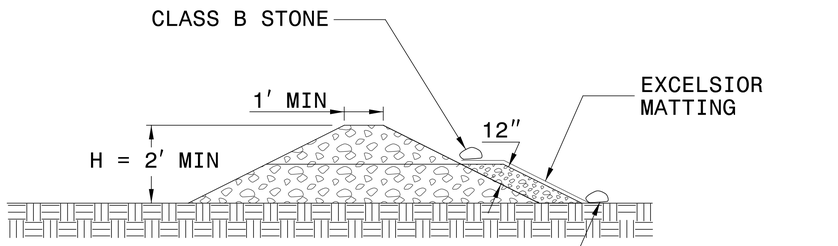
INITIALLY APPLY 4 OUNCES OF POLYACRYLAMIDE (PAM) TO TOP OF MATTING SECTION AND AFTER EVERY RAINFALL EVENT THAT EQUALS OR EXCEEDS 0.50 INCHES.



INSET A



SECTION A-A



SECTION B-B

NOT TO SCALE

PROJECT REFERENCE NO.	SHEET NO.
17BP.7.R.102	EC-3

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

SOIL STABILIZATION TIMEFRAMES

SITE DESCRIPTION	STABILIZATION TIME	TIMEFRAME EXCEPTIONS
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	EC-04/CONST.04

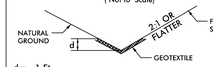
ROADSIDE ENVIRONMENTAL PROJECT ENGINEER

LEVEL III CERTIFIED BY:
STACEY H. BAILEY, PE
CERTIFICATION NUMBER: 3074
ISSUED: OCTOBER 20, 2016

ICA Engineering
5121 Kingdom Way, Suite 100
Raleigh, NC 27607
NC License No: P0058

CLEARING AND GRUBBING EROSION CONTROL FOR CONSTRUCTION SHEET 04

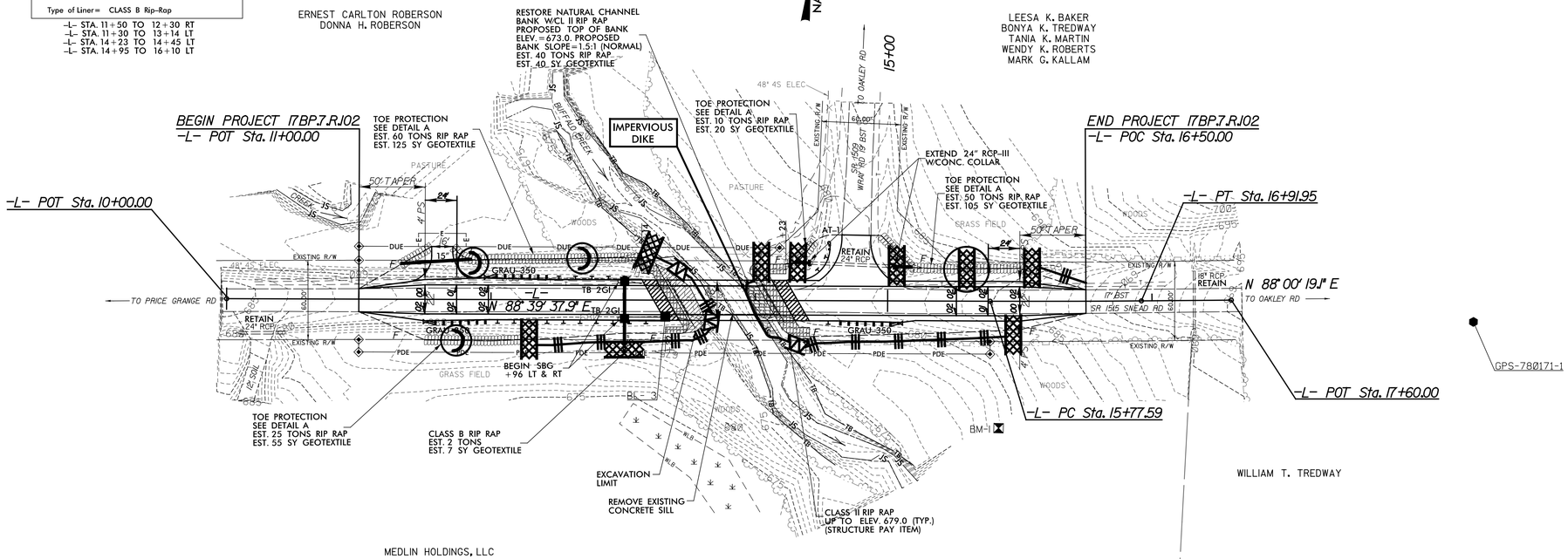
DETAIL A
TOE PROTECTION
(Not to Scale)



d = 1 Ft.

Type of Liner = CLASS B Rip-Rap

-L- STA. 11+50 TO 12+30 RT
-L- STA. 11+30 TO 13+14 LT
-L- STA. 14+23 TO 14+45 LT
-L- STA. 14+95 TO 16+10 LT



NOTE:
PLACE TEMPORARY ROCK SEDIMENT DAMS TYPE – B AND TEMPORARY ROCK SILT CHECKS TYPE – A AT DRAINAGE OUTLETS.

NOTE:
PERIMETER EROSION CONTROL MEASURES SHALL BE INSTALLED DURING CLEARING AND GRUBBING PHASE.

NOTE:
ALL EROSION CONTROL DEVICES SHOWN ARE LOCATED WITHIN EXISTING R/W OR EASEMENT.

ICA ENGINEERING, INC.
R:\Hydraulics\Erosion Control\cadd\780171_hyd_erosion_c&g_psh04.dgn
$$DATE$$ 11/14/2016

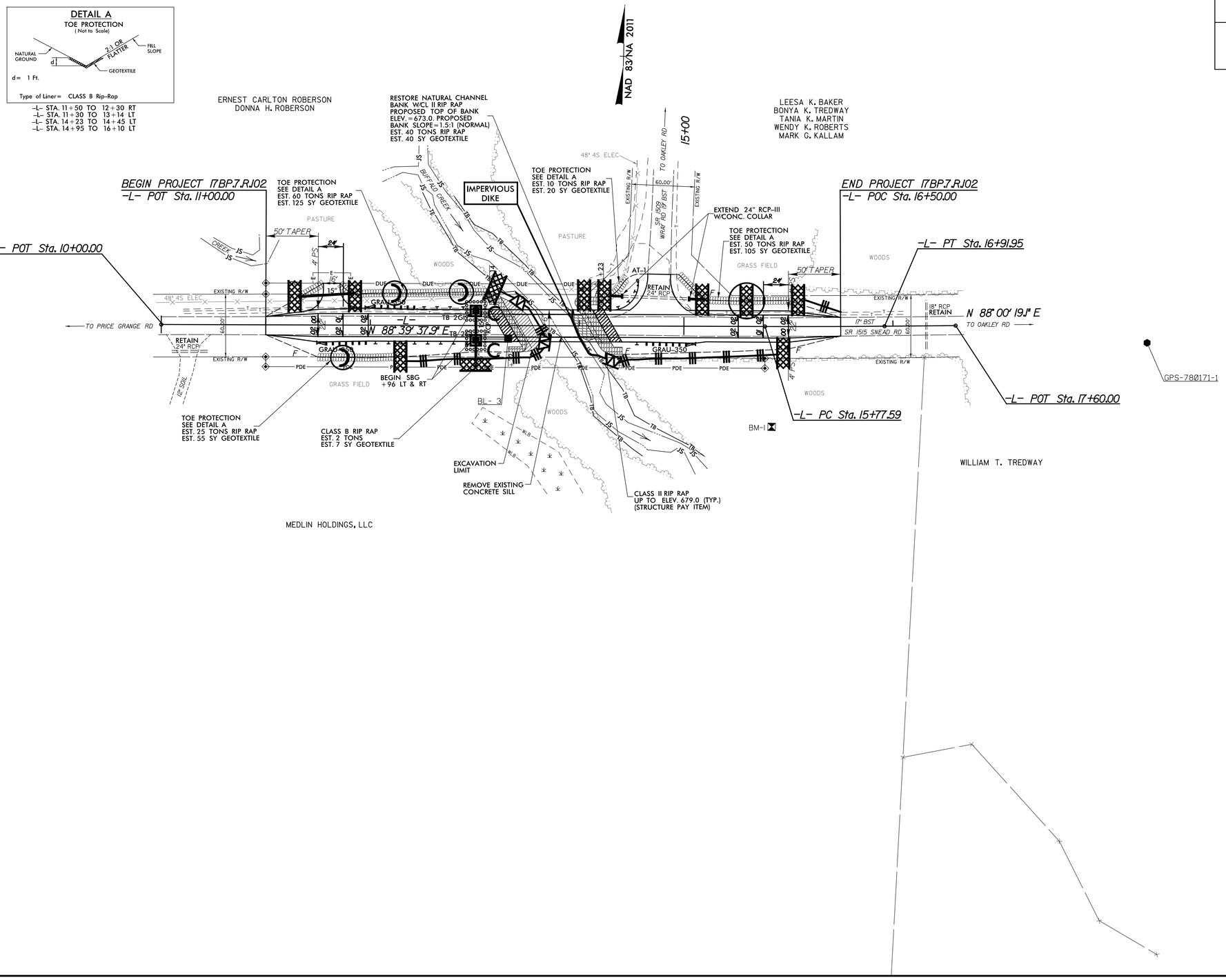
PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	EC-05/CONST.04

ROADSIDE ENVIRONMENTAL PROJECT ENGINEER

LEVEL III CERTIFIED BY:
STACEY H. BAILEY, PE
CERTIFICATION NUMBER: 3074
ISSUED: OCTOBER 20, 2016

ICA Engineering
5121 Kingdom Way, Suite 100
Raleigh, NC 27607
NC License No: P0058

FINAL EROSION CONTROL FOR CONSTRUCTION SHEET 04



ICA ENGINEERING, INC.
R:\Hydraulics\Erosion Control\cadd\780171_hyd_erosion_final_psh04.dgn
$$DATE$$ 11/14/2016

PROJECT REFERENCE	SHEET NO.
17BP.7.R.102 – ROCKINGHAM 171	UO–1

Prepared in the Office of:

THE WOOTEN COMPANY
LICENSE NO. F-0115

THE WOOTEN COMPANY
ENGINEERING | PLANNING | ARCHITECTURE
120 North Boylan Avenue Raleigh NC 27603-1423
919.828.0531 fax 919.834.3589

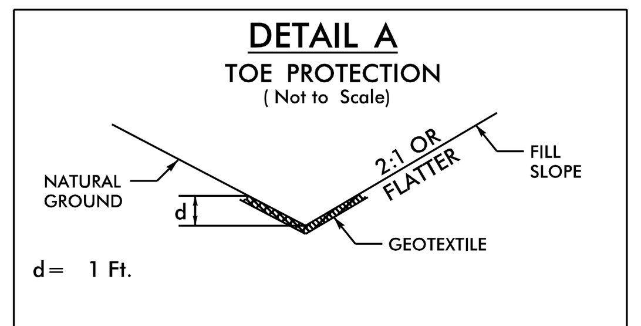
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

GRAPHIC SCALE
25' 0 25' 50'

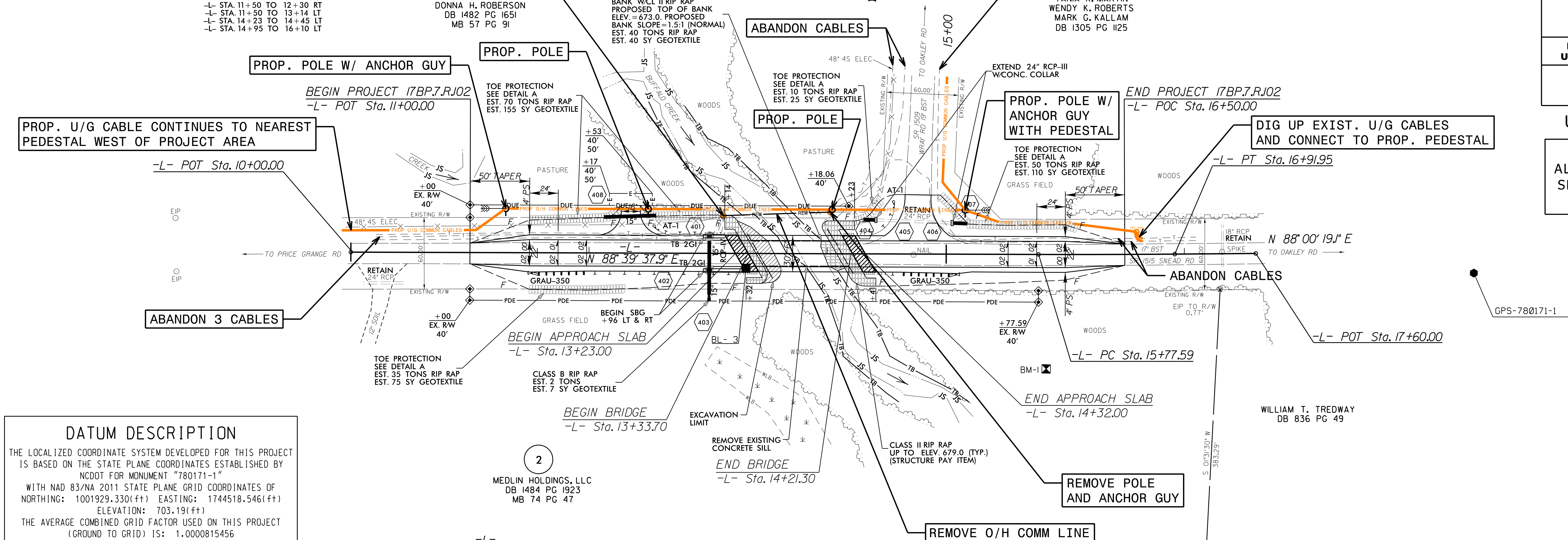
UTILITIES BY OTHERS

NOTE:
ALL PROPOSED UTILITY WORK SHOWN ON THIS SHEET WILL BE DONE BY OTHERS

*DESIGN EXCEPTION:
SAG VERTICAL CURVE K
VERTICAL SSD



–L– STA. 11+50 TO 12+30 RT
–L– STA. 11+50 TO 13+14 LT
–L– STA. 14+23 TO 14+45 LT
–L– STA. 14+95 TO 16+10 LT



DATUM DESCRIPTION

THE LOCALIZED COORDINATE SYSTEM DEVELOPED FOR THIS PROJECT IS BASED ON THE STATE PLANE COORDINATES ESTABLISHED BY NCDOT FOR MONUMENT "780171-1"
WITH NAD 83/NA 2011 STATE PLANE GRID COORDINATES OF
NORTHING: 1001929.330(ft) EASTING: 1744518.546(ft)
ELEVATION: 703.19(ft)
THE AVERAGE COMBINED GRID FACTOR USED ON THIS PROJECT (GROUND TO GRID) IS: 1.0000815456
THE N.C. LAMBERT GRID BEARING AND LOCALIZED HORIZONTAL GROUND DISTANCE FROM "780171-1" TO -L- STATION 10+00.00 IS
S 89° 32' 11.0" W Distance 943.28(ft)
ALL LINEAR DIMENSIONS ARE LOCALIZED HORIZONTAL DISTANCES
VERTICAL DATUM USED IS NAVD 88

-L-
PI Sta 16+34.77
Δ = 0° 39' 18.8" (LT)
D = 0° 34' 22.6"
L = 114.36'
T = 57.18'
R = 10,000.00'

NOTE:
TYPE–III ANCHOR UNITS ON ALL FOUR BRIDGE CORNERS

BM1 ELEVATION • 679.53
N 1001839 E 1744162
BL STATION 7•53.00 83 RIGHT
NAIL IN 22" POPLAR

BL POINT	DESC.	NORTH	EAST	ELEVATION	BL STATION
3	BL-3	1001917.0095	1743907.3174	680.13	5•00.00
1	780171-1	1001929.3300	1744518.5460	703.19	11•11.35
2	780171-2	1002215.9660	1745099.8780	732.61	17•59.51

10/19/2016
2:34:12 PM
T:\Projects\Hatch_Mott_MacDonald-3160\D\17BP7R102_Rockingham#171\CADD\Ub0.dgn
jharris

0 55 100	PROJECT REFERENCE	SHEET NO.
	17BP.7.R.102 – ROCKINGHAM 171	X–1

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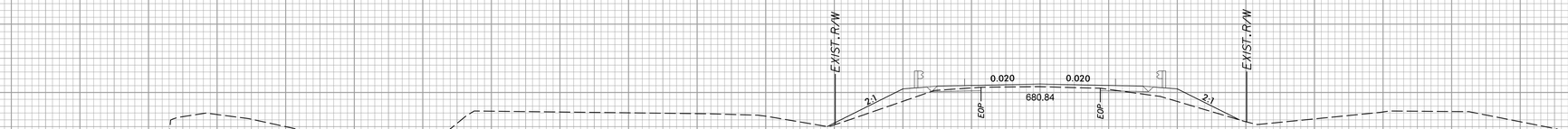
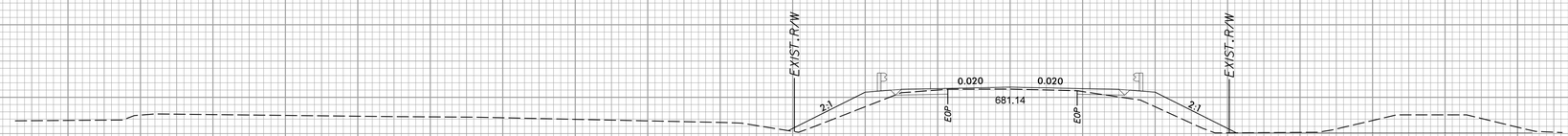
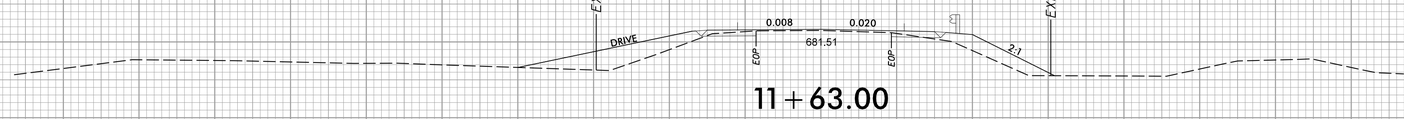
690 680 670



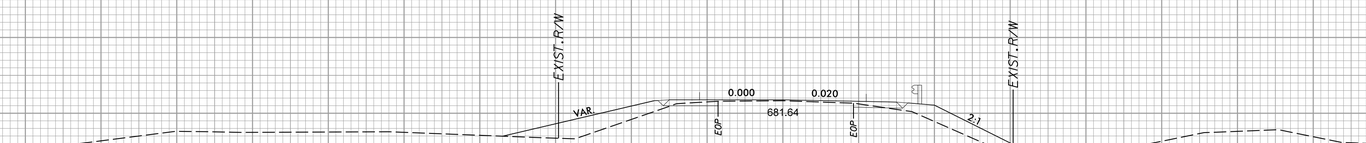
12+50.00



12+00.00



11+63.00



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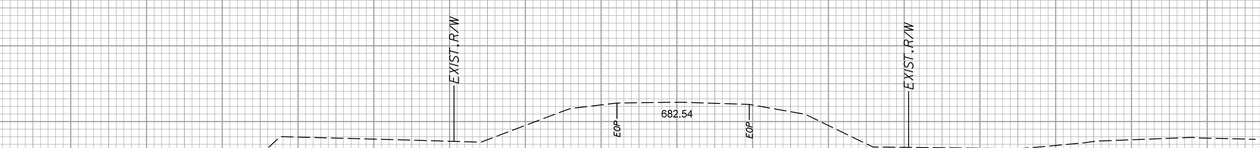

11+00.00

11/14/2016 8:18:53 AM
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jor66165

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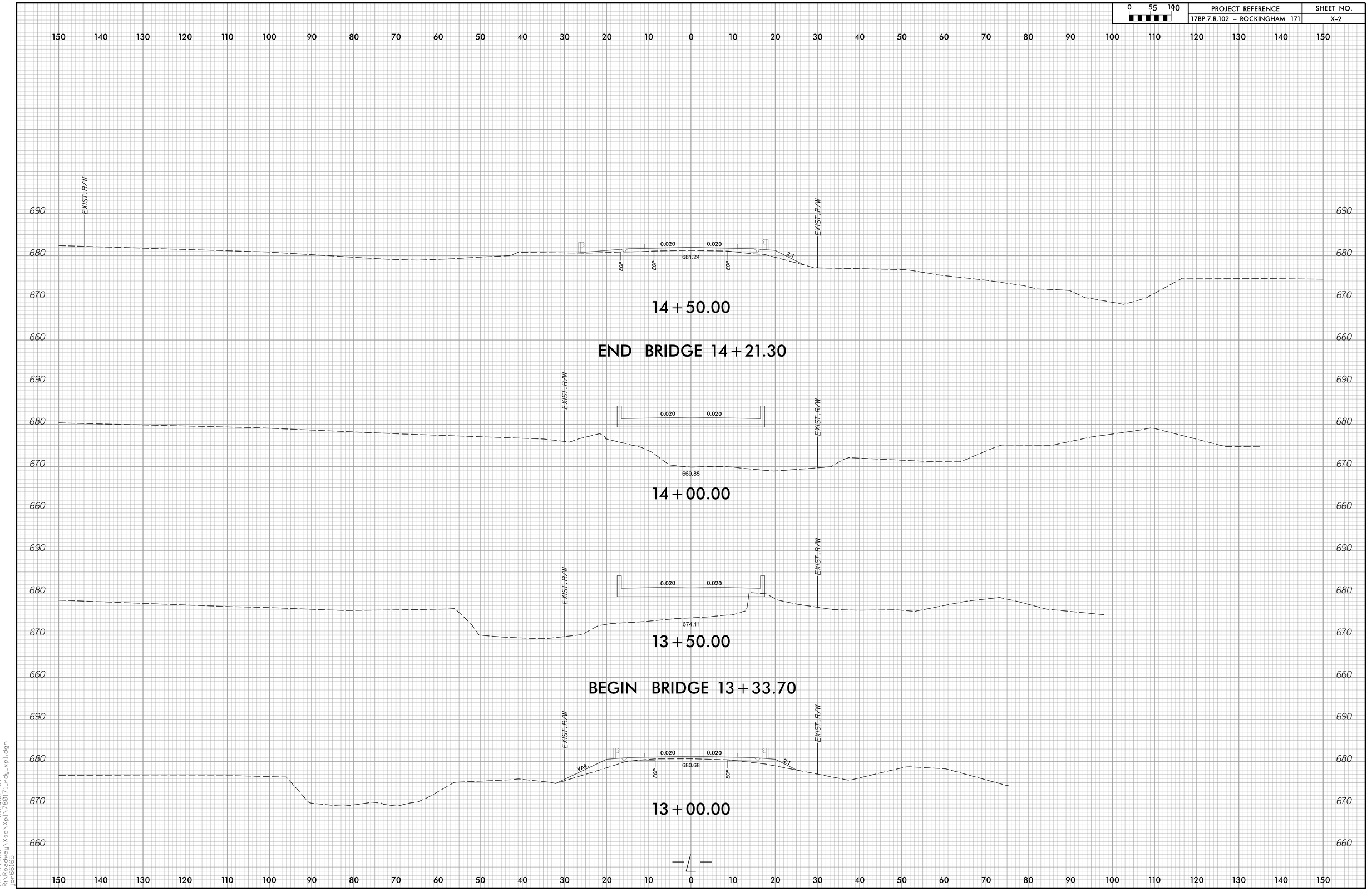
	17BP.7.R.102 – ROCKINGHAM 171	X–2



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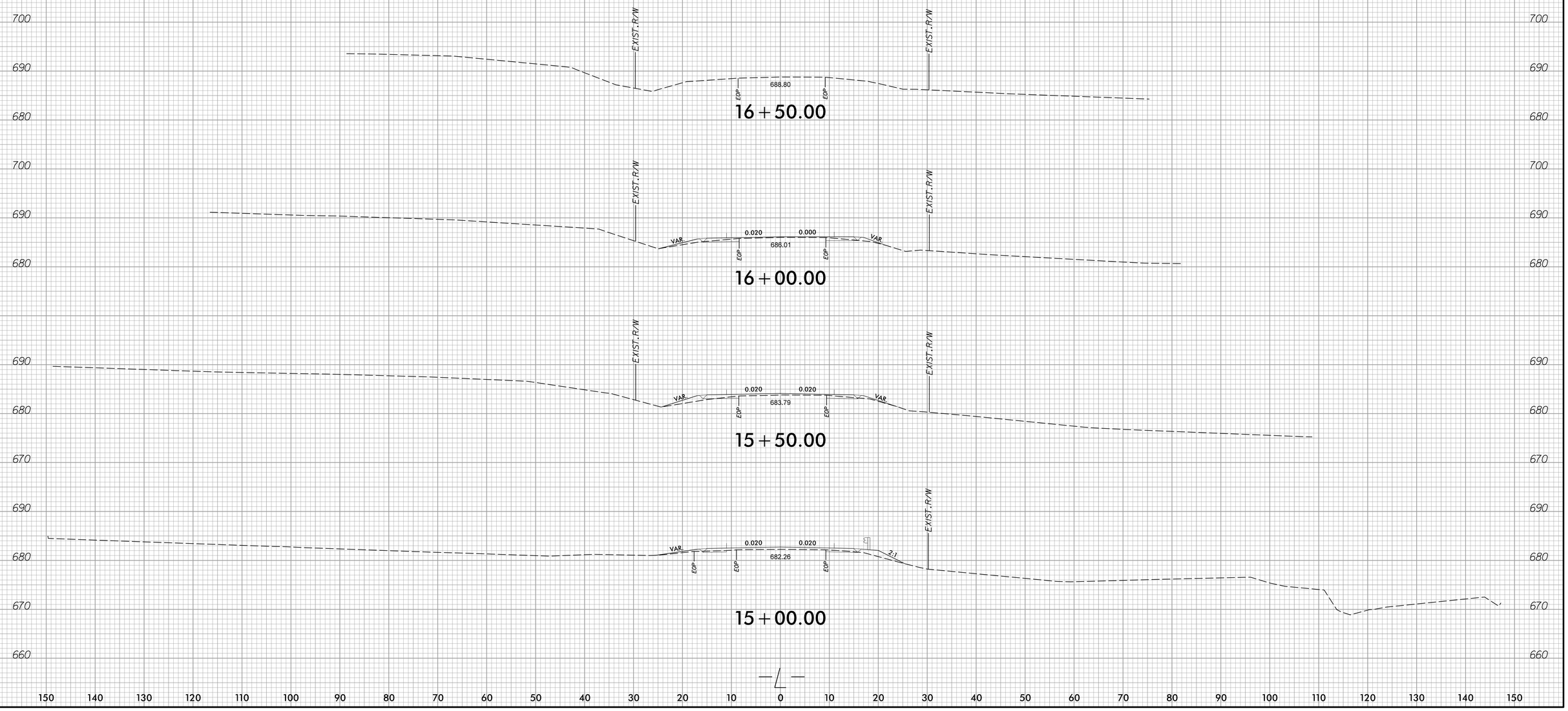

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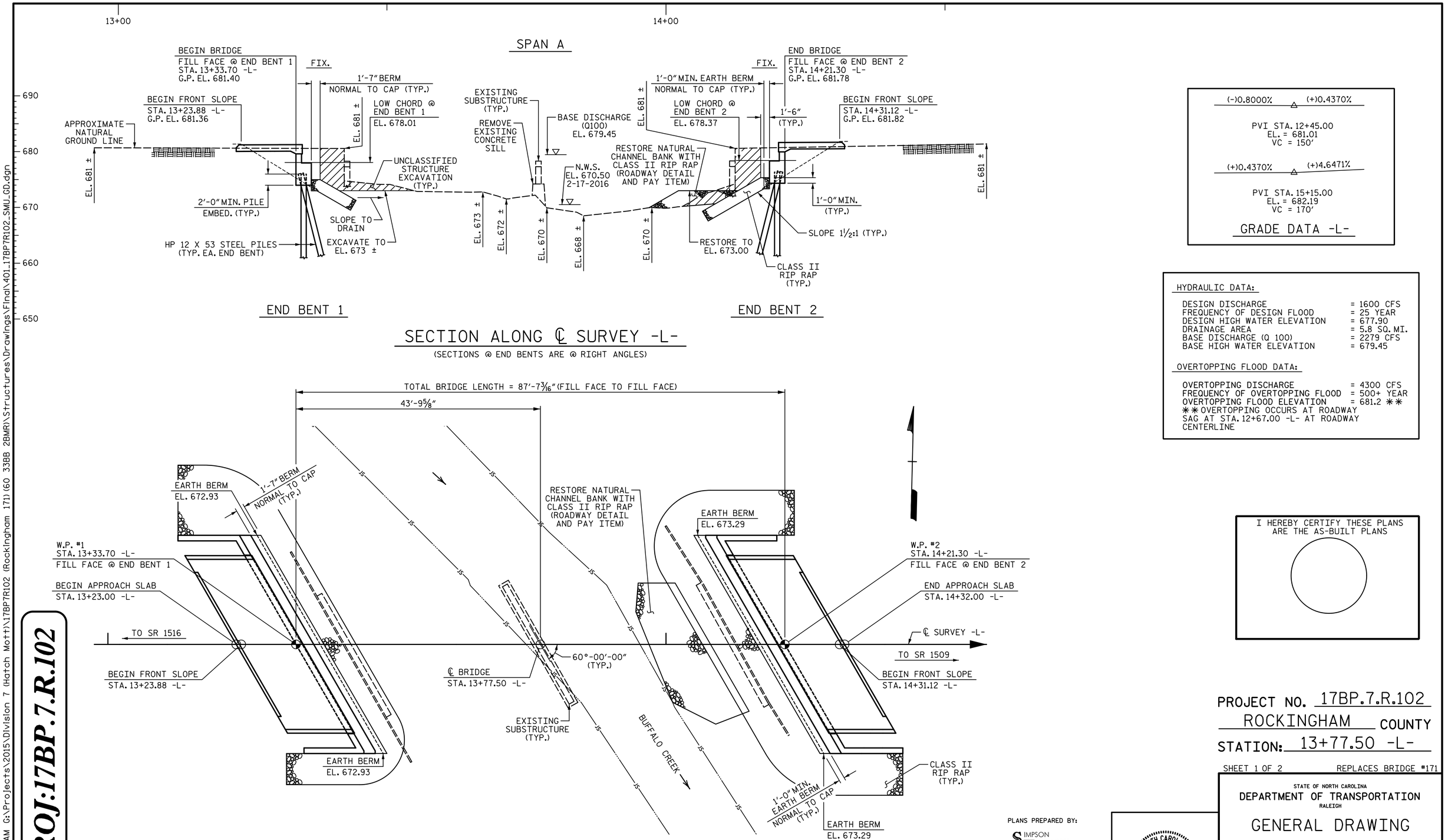


SECTION ALONG ℄ SURVEY -L-

(SECTIONS @ END BENTS ARE @ RIGHT ANGLES)

PLAN

(PILES NOT SHOWN IN PLAN VIEW)

(-)0.8000% (+)0.4370%

PVI STA. 12+45.00
EL. = 681.01
VC = 150'

(+)0.4370% (+)4.6471%

PVI STA. 15+15.00
EL. = 682.19
VC = 170'

GRADE DATA -L-

HYDRAULIC DATA:

DESIGN DISCHARGE	= 1600 CFS
FREQUENCY OF DESIGN FLOOD	= 25 YEAR
DESIGN HIGH WATER ELEVATION	= 677.90
DRAINAGE AREA	= 5.8 SQ. MI.
BASE DISCHARGE (Q 100)	= 2279 CFS
BASE HIGH WATER ELEVATION	= 679.45

OVERTOPPING FLOOD DATA:

OVERTOPPING DISCHARGE	= 4300 CFS
FREQUENCY OF OVERTOPPING FLOOD	= 500+ YEAR
OVERTOPPING FLOOD ELEVATION	= 681.2 **

** OVERTOPPING OCCURS AT ROADWAY SAG AT STA. 12+67.00 -L- AT ROADWAY CENTERLINE

I HEREBY CERTIFY THESE PLANS ARE THE AS-BUILT PLANS

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 1 OF 2 REPLACES BRIDGE #171

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING

FOR BRIDGE ON SR 1515
(SNEAD RD.) OVER
BUFFALO CREEK
BETWEEN SR 1516 AND SR 1509
30'-6" CLEAR ROADWAY - 60° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-1
1			3			TOTAL SHEETS
2			4			19

PROJ:17BP.7.R.102

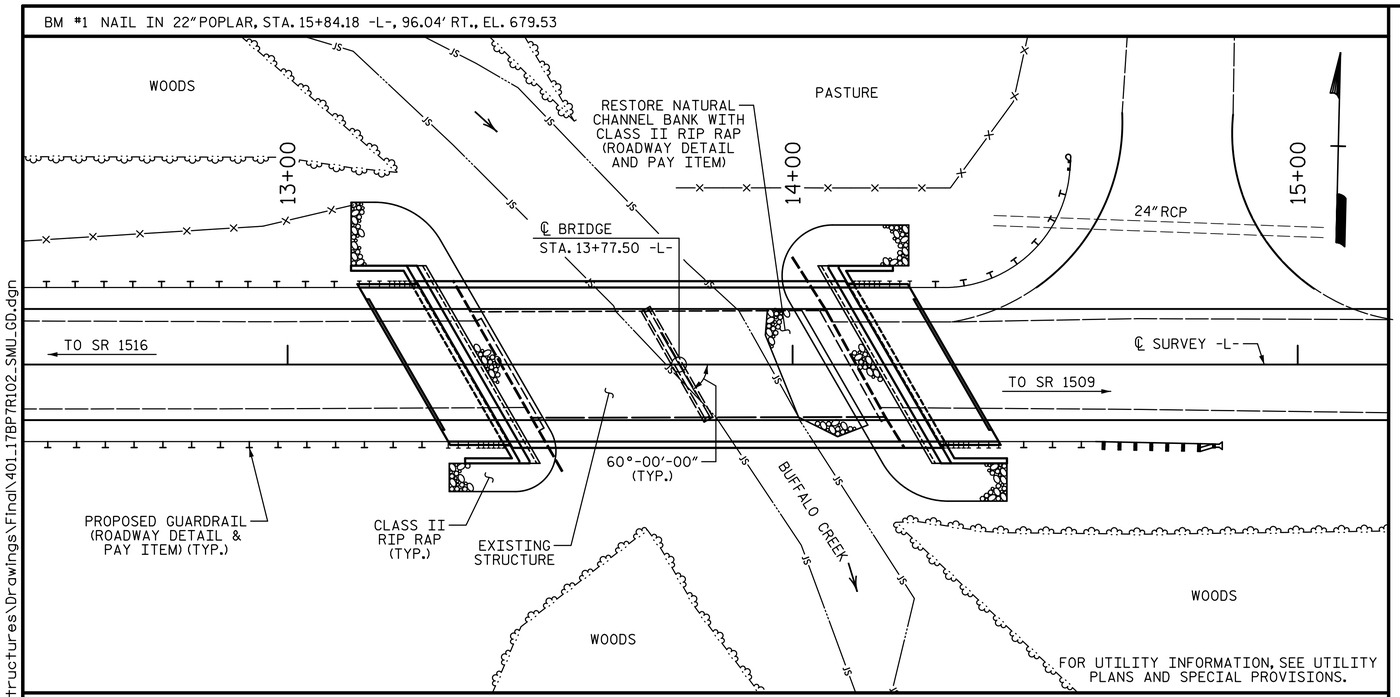
DRAWN BY: T. BANKOVICH DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

PLANS PREPARED BY:

SIMPSON ENGINEERS & ASSOCIATES
5640 Dillard Drive
Suite 200
Cary, NC 27518
(919) 852-0468
(919) 852-0598 (Fax)
www.simpsonengr.com
LICENSURE NO. C-2521

NORTH CAROLINA PROFESSIONAL SEAL 21268 ENGINEER BETSY S. COX
11/1/2016

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



LOCATION SKETCH

NOTES:

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING.

THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.

THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

FOR EROSION CONTROL MEASURES, SEE EROSION CONTROL PLANS.

REMOVAL OF THE EXISTING BRIDGE SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL INTO THE WATER. THE CONTRACTOR SHALL REMOVE THE BRIDGE AND SUBMIT PLANS FOR DEMOLITION IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS.

THE MATERIAL SHOWN IN THE CROSS-HATCHED AREA SHALL BE EXCAVATED FOR A DISTANCE OF 30 FT. LEFT AND 25 FT. RIGHT OF CENTERLINE ROADWAY AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR UNCLASSIFIED STRUCTURE EXCAVATION. SEE SECTION 412 OF THE STANDARD SPECIFICATIONS.

THE EXISTING STRUCTURE CONSISTS OF 2 SPANS @ 35'-0". THE SUPERSTRUCTURE HAS A CLEAR ROADWAY WIDTH OF 19'-2" WITH A TIMBER DECK ON STEEL I-BEAMS. THE END BENTS CONSIST OF STEEL CAPS AND PILES. THE INTERIOR BENT CONSISTS OF STEEL CAP AND CONCRETE ENCASED STEEL PILES. THE EXISTING STRUCTURE WHICH IS LOCATED AT THE SITE OF THE PROPOSED STRUCTURE SHALL BE REMOVED. THE EXISTING BRIDGE IS PRESENTLY POSTED FOR LOAD LIMIT. SHOULD THE STRUCTURAL INTEGRITY OF THE BRIDGE DETERIORATE DURING CONSTRUCTION OF THE PROPOSED BRIDGE, THE LOAD LIMIT MAY BE REDUCED AS NECESSARY DURING THE LIFE OF THE PROJECT.

REMOVE EXISTING SILL AT INTERIOR BENT.

THE SUBSTRUCTURE OF THE EXISTING BRIDGE INDICATED ON THE PLANS IS FROM THE BEST INFORMATION AVAILABLE. SINCE THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THE EXISTING BRIDGE SUBSTRUCTURE SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

THIS STRUCTURE HAS BEEN DESIGNED IN ACCORDANCE WITH "HEC 18-EVALUATING SCOUR AT BRIDGES."

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

ASPHALT WEARING SURFACE IS INCLUDED IN ROADWAY QUANTITY ON ROADWAY PLANS.

INASMUCH AS THE PAINT SYSTEM ON THE EXISTING STRUCTURAL STEEL CONTAINS LEAD, THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLE 107-1 OF THE STANDARD SPECIFICATIONS. ANY COSTS RESULTING FROM COMPLIANCE WITH APPLICABLE STATE OR FEDERAL REGULATIONS PERTAINING TO HANDLING OF MATERIALS CONTAINING LEAD BASED PAINT SHALL BE INCLUDED IN THE BID PRICE FOR "REMOVAL OF EXISTING STRUCTURE AT STATION 13+77.50 -L-."

FOR ASBESTOS ASSESSMENT FOR BRIDGE DEMOLITION AND RENOVATION ACTIVITIES, SEE SPECIAL PROVISIONS.

FOUNDATION NOTES:

FOR PILES, SEE GEOTECHNICAL SPECIAL PROVISIONS AND SECTION 450 OF THE STANDARD SPECIFICATIONS.

PILES AT END BENT 1 ARE DESIGNED FOR A FACTORED RESISTANCE OF 100 TONS PER PILE.

DRIVE PILES AT END BENT 1 TO A REQUIRED DRIVING RESISTANCE OF 170 TONS PER PILE.

STEEL H-PILE POINTS ARE REQUIRED FOR STEEL H-PILES AT END BENT 1. FOR STEEL PILE POINTS, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

PILES AT END BENT 2 ARE DESIGNED FOR A FACTORED RESISTANCE OF 100 TONS PER PILE.

DRIVE PILES AT END BENT 2 TO A REQUIRED DRIVING RESISTANCE OF 170 TONS PER PILE.

STEEL H-PILE POINTS ARE REQUIRED FOR STEEL H-PILES AT END BENT 2. FOR STEEL PILE POINTS, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

DRILLED-IN-PILES MAY BE REQUIRED FOR END BENT 2. IF REQUIRED, EXCAVATE HOLES AT PILE LOCATIONS TO ELEVATION 664.5 FT, AND WITH A MINIMUM OF 5 FT EMBEDMENT INTO WEATHERED ROCK OR ROCK. FOR PILE EXCAVATION, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

CONCRETE OR GROUT IS REQUIRED TO FILL HOLES FOR PILE EXCAVATION AT END BENT 2.

TOTAL BILL OF MATERIAL

	REMOVAL OF EXISTING STRUCTURE	PILE EXCAVATION IN SOIL	PILE EXCAVATION NOT IN SOIL	UNCLASSIFIED STRUCTURE EXCAVATION	CLASS A CONCRETE	BRIDGE APPROACH SLABS	REINFORCING STEEL	HP 12 X 53 STEEL PILES		STEEL PILE POINTS	TWO BAR METAL RAIL	1'-2" X 2'-9½" CONCRETE PARAPET	1'-2" X 2'-10½" CONCRETE PARAPET	RIP RAP CLASS II (2'-0" THICK)	GEOTEXTILE FOR DRAINAGE	ELASTOMERIC BEARINGS	3'-0" X 2'-9" PRESTRESSED CONCRETE BOX BEAMS		ASBESTOS ASSESSMENT
	LS	LF	LF	LS	CY	LS	LB	NO.	LF	EA	LF	LF	LF	TON	SY	LS	NO.	LF	LS
SUPERSTRUCTURE						LS					153.65	85	85				11	935.00	
END BENT 1				LS	28.5		3,934	7	175	7				105	120				
END BENT 2		15	10	LS	28.5		3,934	7	140	7				130	145				
TOTAL	LS	15	10	LS	57.0	LS	7,868	14	315	14	153.65	85	85	235	265	LS	11	935.00	LS

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 2 OF 2

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING

FOR BRIDGE ON SR 1515
(SNEAD RD.) OVER
BUFFALO CREEK
BETWEEN SR 1516 AND SR 1509
30'-6" CLEAR ROADWAY - 60° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-2
1			3			TOTAL SHEETS
2			4			19

PLANS PREPARED BY:

SIMPSON ENGINEERS & ASSOCIATES
5640 Dillard Drive
Suite 200
Cary, NC 27518
(919) 852-0468
(919) 852-0598 (Fax)
www.simpsonengr.com
LICENSURE NO. C-2521

NORTH CAROLINA PROFESSIONAL SEAL 031268 ENGINEER BETSY S. COX
11/1/2016

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

DRAWN BY: S.D. COOPER DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

LOAD AND RESISTANCE FACTOR RATING (LRFD) SUMMARY FOR PRESTRESSED CONCRETE GIRDERS

LEVEL		VEHICLE	WEIGHT (W) (TONS)	CONTROLLING LOAD RATING	MINIMUM RATING FACTORS (RF)	TONS = W X RF	STRENGTH I LIMIT STATE										SERVICE III LIMIT STATE						COMMENT NUMBER
								MOMENT					SHEAR					MOMENT						
							LIVELOAD FACTORS	DISTRIBUTION FACTORS (DF)	RATING FACTOR	SPAN	GIRDER LOCATION	DISTANCE FROM LEFT END OF SPAN (ft)	DISTRIBUTION FACTORS (DF)	RATING FACTOR	SPAN	GIRDER LOCATION	DISTANCE FROM LEFT END OF SPAN (ft)	LIVELOAD FACTORS	DISTRIBUTION FACTORS (DF)	RATING FACTOR	SPAN	GIRDER LOCATION	DISTANCE FROM LEFT END OF SPAN (ft)	
DESIGN LOAD RATING		HL-93(Inv)	N/A	**1**	1.117	--	1.75	0.247	1.93	A	EL	41.634	0.625	**1.12**	A	EL	**8.327**	0.80	0.247	1.56	A	EL	41.634	
		HL-93(Opr)	N/A	--	1.448	--	1.35	0.247	2.5	A	EL	41.634	0.625	1.45	A	EL	8.327	N/A	--	--	--	--	--	
		HS-20(Inv)	36.000	**2**	1.448	52.14	1.75	0.247	2.59	A	EL	41.634	0.625	**1.45**	A	EL	**8.327**	0.80	0.247	2.09	A	EL	41.634	
		HS-20(Opr)	36.000	--	1.877	67.589	1.35	0.247	3.35	A	EL	41.634	0.625	1.88	A	EL	8.327	N/A	--	--	--	--	--	
LEGAL LOAD RATING	SV	SNSH	13.500	--	4.378	59.102	1.4	0.247	7.48	A	EL	41.634	0.625	4.38	A	EL	8.327	0.80	0.247	4.84	A	EL	41.634	
		SNGARBS2	20.000	--	3.091	61.822	1.4	0.247	5.5	A	EL	41.634	0.625	3.09	A	EL	8.327	0.80	0.247	3.56	A	EL	41.634	
		SNAGRIS2	22.000	--	2.861	62.937	1.4	0.247	5.17	A	EL	41.634	0.625	2.86	A	EL	8.327	0.80	0.247	3.35	A	EL	41.634	
		SNCOTTS3	27.250	--	2.183	59.498	1.4	0.247	3.72	A	EL	41.634	0.625	2.18	A	EL	8.327	0.80	0.247	2.41	A	EL	41.634	
		SNAGGRS4	34.925	--	1.797	62.749	1.4	0.247	3.08	A	EL	41.634	0.625	1.8	A	EL	8.327	0.80	0.247	1.99	A	EL	41.634	
		SNS5A	35.550	--	1.812	64.409	1.4	0.247	3.01	A	EL	41.634	0.625	1.81	A	EL	8.327	0.80	0.247	1.95	A	EL	41.634	
		SNS6A	39.950	--	1.647	65.797	1.4	0.247	2.75	A	EL	41.634	0.625	1.65	A	EL	8.327	0.80	0.247	1.78	A	EL	41.634	
		SNS7B	42.000	--	1.61	67.634	1.4	0.247	2.62	A	EL	41.634	0.625	1.61	A	EL	8.327	0.80	0.247	1.70	A	EL	41.634	
	TTST	TNAGRIT3	33.000	--	1.965	64.845	1.4	0.247	3.35	A	EL	41.634	0.625	1.97	A	EL	8.327	0.80	0.247	2.17	A	EL	41.634	
		TNT4A	33.075	--	1.922	63.556	1.4	0.247	3.36	A	EL	41.634	0.625	1.92	A	EL	8.327	0.80	0.247	2.18	A	EL	41.634	
		TNT6A	41.600	--	1.701	70.755	1.4	0.247	2.74	A	EL	41.634	0.625	1.7	A	EL	8.327	0.80	0.247	1.77	A	EL	41.634	
		TNT7A	42.000	--	1.67	70.125	1.4	0.247	2.75	A	EL	41.634	0.625	1.67	A	EL	8.327	0.80	0.247	1.78	A	EL	41.634	
		TNT7B	42.000	--	1.578	66.274	1.4	0.247	2.83	A	EL	41.634	0.625	1.58	A	EL	8.327	0.80	0.247	1.83	A	EL	41.634	
		TNAGRIT4	43.000	--	1.53	65.773	1.4	0.247	2.7	A	EL	41.634	0.625	1.53	A	EL	8.327	0.80	0.247	1.75	A	EL	41.634	
		TNAGT5A	45.000	--	1.511	68.008	1.4	0.247	2.55	A	EL	41.634	0.625	1.51	A	EL	8.327	0.80	0.247	1.65	A	EL	41.634	
		TNAGT5B	45.000	**3**	1.456	65.508	1.4	0.247	2.52	A	EL	41.634	0.625	**1.46**	A	EL	**8.327**	0.80	0.247	1.63	A	EL	41.634	

LOAD FACTORS:

DESIGN LOAD RATING FACTORS	LIMIT STATE	γ_{DC}	γ_{DW}
	STRENGTH I	1.25	1.50
	SERVICE III	1.00	1.00

NOTES:

MINIMUM RATING FACTORS ARE BASED ON THE STRENGTH I AND SERVICE III LIMIT STATES.

ALLOWABLE STRESSES FOR SERVICE III LIMIT STATE ARE AS REQUIRED FOR DESIGN.

DISTANCE FROM LEFT END OF SPAN IS MEASURED FROM ℄ BEARING.

(#) CONTROLLING LOAD RATING

(1) DESIGN LOAD RATING (HL-93)

(2) DESIGN LOAD RATING (HS-20)

(3) LEGAL LOAD RATING **

** SEE CHART FOR VEHICLE TYPE

GIRDER LOCATION

I - INTERIOR GIRDER
EL - EXTERIOR LEFT GIRDER
ER - EXTERIOR RIGHT GIRDER

LRFR SUMMARY

SPAN A

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

PLANS PREPARED BY:
SIMPSON ENGINEERS & ASSOCIATES
5640 Dillard Drive
Suite 200
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(919) 852-0468
(919) 852-0598 (Fax)
www.simpsonengr.com
LICENSURE NO. C-2521

NORTH CAROLINA PROFESSIONAL SEAL 031268 ENGINEER BETSY S. COX
11/1/2016

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

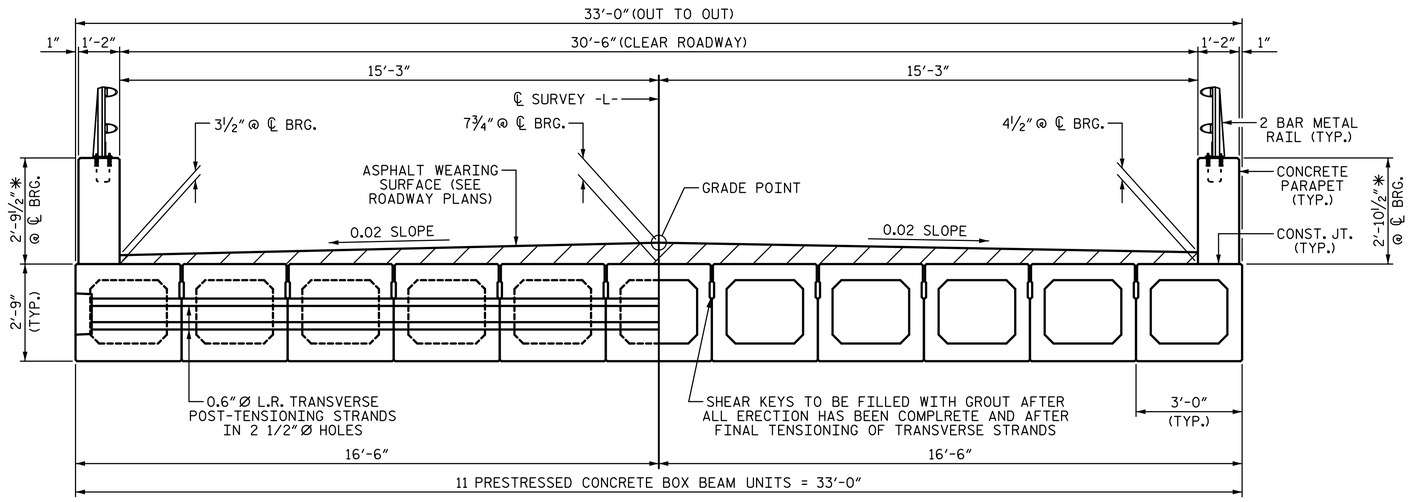
DRAWN BY: S.D. COOPER DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

LRFR SUMMARY FOR 85'-0" BOX BEAM UNIT 60° SKEW

(NON-INTERSTATE TRAFFIC)

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-3
1			3			TOTAL SHEETS
2			4			19

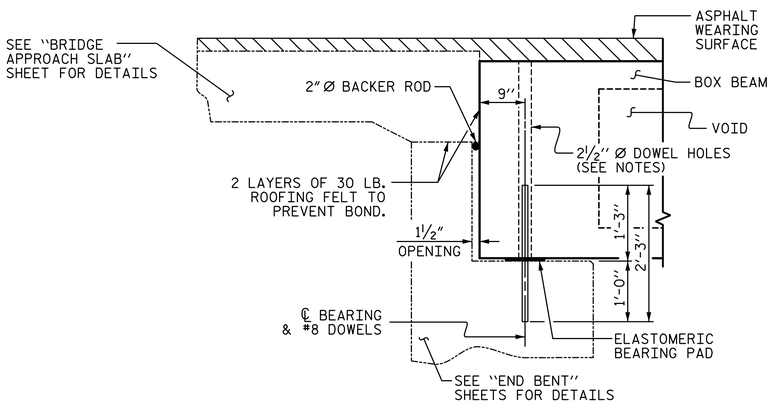


HALF SECTION
(AT INTERMEDIATE DIAPHRAGMS)

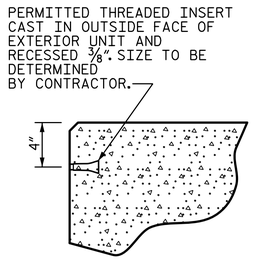
HALF SECTION
(THROUGH VOIDS)

TYPICAL SECTION

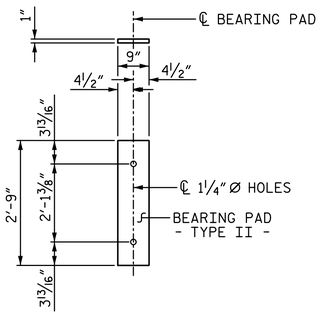
*- THE MAXIMUM CONCRETE PARAPET HEIGHTS AND ASPHALT THICKNESS ARE SHOWN. THE HEIGHT OF THE CONCRETE PARAPET AND ASPHALT THICKNESS VARIES WHILE THE TOP OF THE CONCRETE PARAPET FOLLOWS THE PROFILE OF THE GUTTERLINE.



SECTION AT END BENT



THREADED INSERT DETAIL



FIXED END
(TYPE II - 22 REQ'D)

ELASTOMERIC BEARING DETAILS

ELASTOMER IN ALL BEARINGS SHALL BE 60 DUROMETER HARDNESS.

NOTES:

ALL PRESTRESSING STRANDS SHALL BE 7-WIRE LOW RELAXATION GRADE 270 STRANDS AND SHALL CONFORM TO AASHTO M203 EXCEPT FOR SAMPLING REQUIREMENTS WHICH SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ALL REINFORCING STEEL CAST WITH THE BOX BEAM SECTIONS SHALL BE GRADE 60 AND SHALL BE INCLUDED IN THE UNIT PRICE BID FOR PRESTRESSED CONCRETE BOX BEAMS.

FLAME CUTTING OF THE TRANSVERSE POST-TENSIONING STRAND IS NOT ALLOWED.

RECESSES FOR TRANSVERSE STRANDS SHALL BE GROUTED AFTER THE TENSIONING OF THE STRANDS.

THE 2½"Ø DOWEL HOLES AT FIXED ENDS OF BOX BEAM SECTIONS SHALL BE FILLED WITH NON-SHRINK GROUT.

THE BACKER RODS SHALL CONFORM TO THE REQUIREMENTS OF TYPE M BOND BREAKER. SEE SECTION 1028 OF THE STANDARD SPECIFICATIONS.

THE TRANSFER OF LOAD FROM THE ANCHORAGES TO THE BOX BEAM UNIT SHALL BE DONE WHEN THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF NOT LESS THAN 6000 PSI.

ALL REINFORCING STEEL IN CONCRETE PARAPETS SHALL BE EPOXY COATED.

PRESTRESSING STRANDS SHALL BE CUT FLUSH WITH THE BOX BEAM UNIT ENDS.

APPLY EPOXY PROTECTIVE COATING TO BOX BEAM UNIT ENDS.

VERTICAL GROOVED CONTRACTION JOINTS, ½" IN DEPTH, SHALL BE TOOLED IN ALL EXPOSED FACES OF THE PARAPET AND IN ACCORDANCE WITH ARTICLE 825-10(B) OF THE STANDARD SPECIFICATIONS. A VERTICAL CONTRACTION JOINT SHALL BE LOCATED AT EACH THIRD POINT BETWEEN PARAPET EXPANSION JOINTS. ONLY ONE CONTRACTION JOINT IS REQUIRED AT MIDPOINT OF PARAPET SEGMENTS LESS THAN 20 FEET IN LENGTH AND NO CONTRACTION JOINTS ARE REQUIRED FOR THOSE SEGMENTS LESS THAN 10 FEET IN LENGTH.

THE LOCATION OF THE VOID DRAINS MAY BE SHIFTED SLIGHTLY WHERE NECESSARY TO CLEAR PRESTRESSING STRANDS OR TRANSVERSE REINFORCING STEEL.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

THE PERMITTED THREADED INSERTS ARE DETAILED AS AN OPTION FOR THE CONTRACTOR TO ATTACH FALSEWORK AND FORMWORK DURING CONSTRUCTION.

THE PERMITTED THREADED INSERTS IN THE EXTERIOR UNITS SHALL BE SIZED BY THE CONTRACTOR, SPACED AT 4'-0" CENTERS AND GALVANIZED IN ACCORDANCE WITH SECTION 1076 OF THE STANDARD SPECIFICATIONS. STAINLESS STEEL THREADED INSERTS MAY BE USED AS AN ALTERNATE.

THE PERMITTED THREADED INSERTS SHALL BE GROUTED BY THE CONTRACTOR IMMEDIATELY FOLLOWING REMOVAL OF THE FALSEWORK.

THE COST OF THE PERMITTED THREADED INSERTS SHALL BE INCLUDED IN THE PRICE BID FOR THE PRECAST UNITS.

PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

SHEET 1 OF 4

DRAWN BY: T. BANKOVICH DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

PLANS PREPARED BY:
SIMPSON ENGINEERS & ASSOCIATES
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LICENSURE NO. C-2521

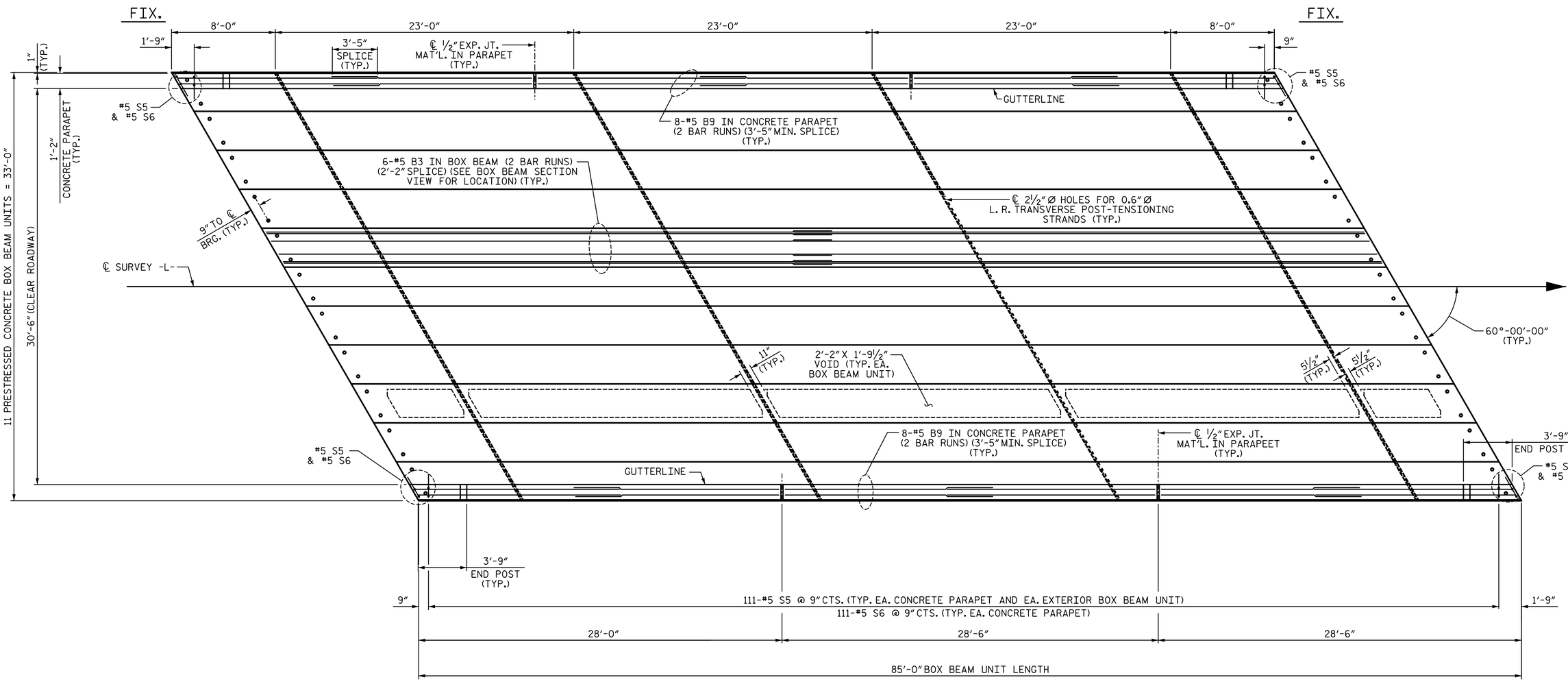
NORTH CAROLINA PROFESSIONAL SEAL 031268 ENGINEER BETSY S. COX
11/1/2016

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

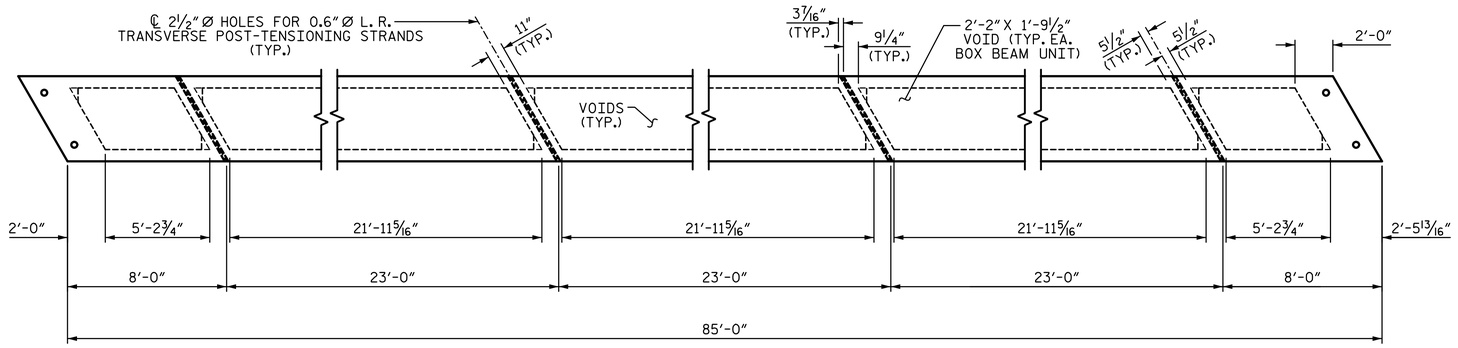
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUPERSTRUCTURE
3'-0" X 2'-9" PRESTRESSED CONCRETE BOX BEAM UNIT
60° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-4
1			3			TOTAL SHEETS
2			4			19



PLAN OF SPAN A



DIAPHRAGM AND VOID LAYOUT

DRAWN BY: T. BANKOVICH DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

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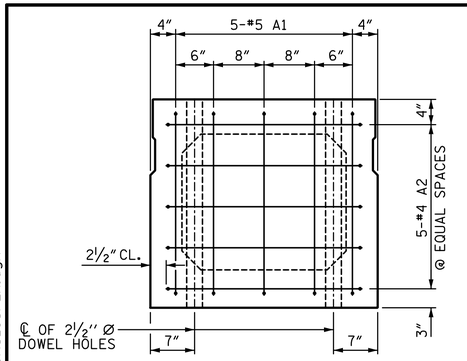
PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

SHEET 2 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE

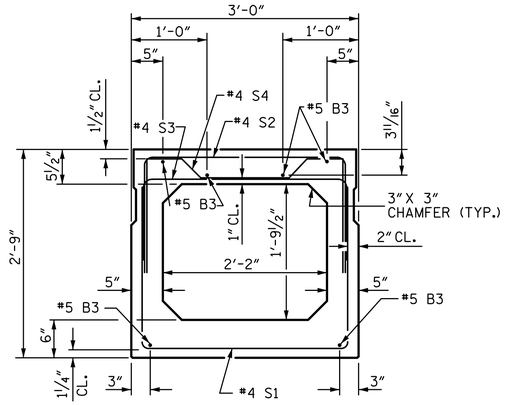
PLAN OF SPAN A (85'-0" UNIT) 30'-6" CLEAR ROADWAY 60° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-5
1			3			TOTAL SHEETS
2			4			19



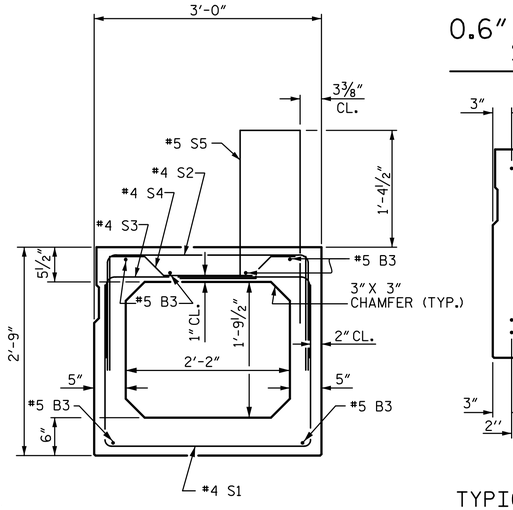
END ELEVATION

SHOWING PLACEMENT OF #5 & #4 "A" BARS AND LOCATION OF DOWEL HOLES. (INTERIOR BOX BEAM SECTION SHOWN-EXTERIOR SECTION SIMILAR EXCEPT SHEAR KEY LOCATION. STRAND LAYOUT NOT SHOWN.)



INTERIOR BOX BEAM SECTION

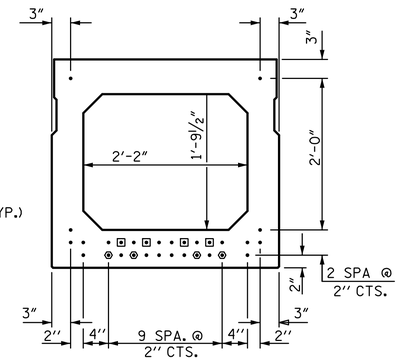
(STRAND LAYOUT NOT SHOWN)



EXTERIOR BOX BEAM SECTION

(STRAND LAYOUT NOT SHOWN)

0.6″ Ø LOW RELAXATION STRAND LAYOUT



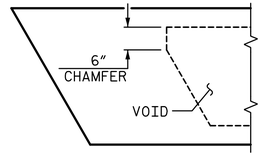
TYPICAL STAND LOCATION

(30 STRANDS REQUIRED)

DEBONDING LEGEND

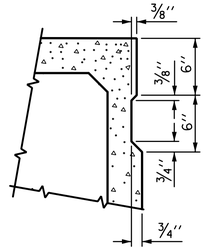
- ● FULLY BONDED STRANDS
- ▣ STRANDS DEBONDED FOR 4′-0″ FROM END OF GIRDER
- ⬣ STRANDS DEBONDED FOR 12′-0″ FROM END OF GIRDER

BOND SHALL BE BROKEN ON STRANDS AS SHOWN FOR THE SPECIFIED LENGTH FROM EACH END OF THE BOX BEAM. SEE STANDARD SPECIFICATIONS ARTICLE 1078-7.



CHAMFER DETAIL

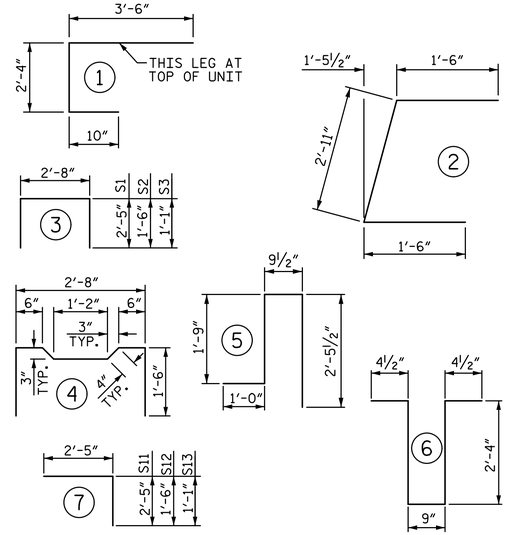
SHOWING 6″ VOID CHAMFER



SHEAR KEY DETAIL

NOTE: OMIT SHEAR KEY ON OUTSIDE FACE OF EXTERIOR BOX BEAMS.

BAR TYPES



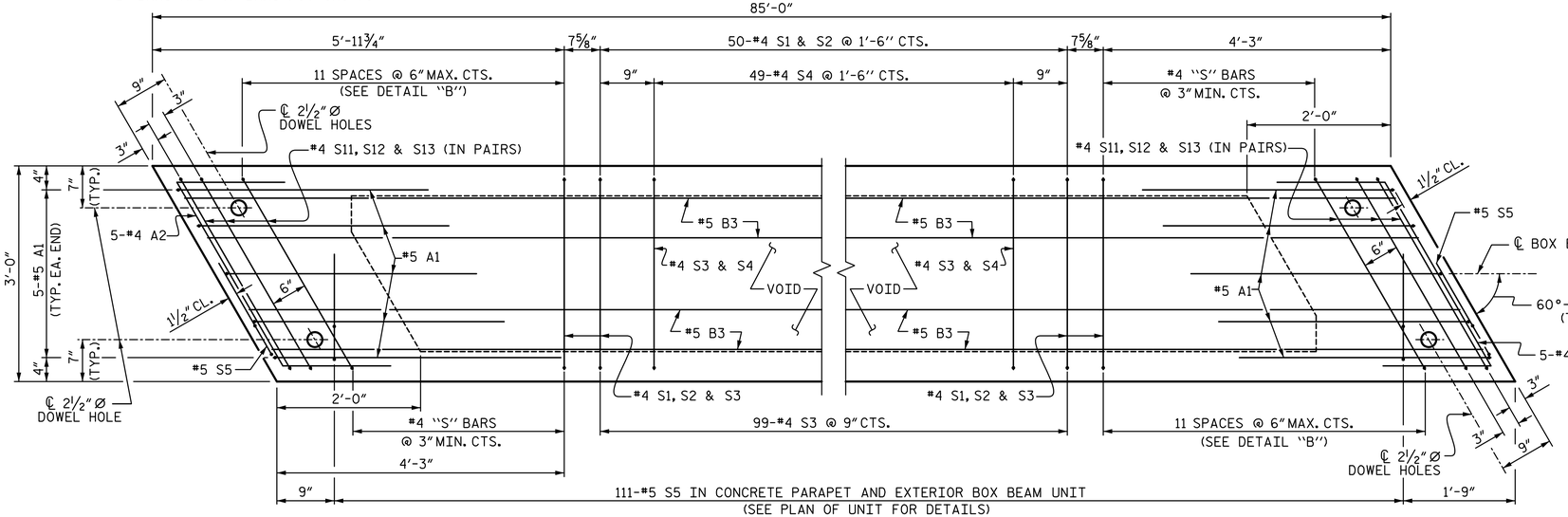
ALL BAR DIMENSIONS ARE OUT TO OUT

BILL OF MATERIAL FOR ONE BOX BEAM SECTION

				EXTERIOR UNIT		INTERIOR UNIT	
BAR	NUMBER	SIZE	TYPE	LENGTH	WEIGHT	LENGTH	WEIGHT
A1	10	#5	1	6′-8″	70	6′-8″	70
A2	34	#4	2	5′-11″	134	5′-11″	134
B3	12	#5	STR	43′-5″	543	43′-5″	543
K1	12	#4	6	6′-2″	49	6′-2″	49
K2	8	#4	STR	2′-10″	15	2′-10″	15
S1	62	#4	3	7′-6″	311	7′-6″	311
S2	62	#4	3	5′-8″	235	5′-8″	235
S3	111	#4	3	4′-10″	358	4′-10″	358
S4	49	#4	4	5′-10″	191	5′-10″	191
S11	32	#4	7	4′-10″	103	4′-10″	103
S12	32	#4	7	3′-11″	84	3′-11″	84
S13	32	#4	7	3′-6″	75	3′-6″	75
✳ S5	113	#5	5	6′-0″	707	--	--
REINFORCING STEEL					2168 LBS.		2168 LBS.
✳ EPOXY COATED REINF. STEEL					707 LBS.		
8000 P.S.I. CONCRETE				15.2	CU. YDS.	15.0	CU. YDS.
0.6″ Ø L.R. STRANDS				No. 30		No. 30	

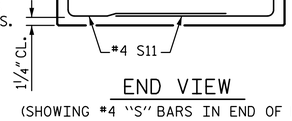
GRADE 270 STRANDS

	0.6″ Ø L.R.
AREA (SQUARE INCHES)	0.217
ULTIMATE STRENGTH (LBS. PER STRAND)	58,600
APPLIED PRESTRESS (LBS. PER STRAND)	43,950



DETAIL "B"

EXTERIOR UNIT SHOWN, INTERIOR UNIT SIMILAR EXCEPT OMIT #5 S5 BARS. "B" BARS AND "A" BARS NOT SHOWN.



END VIEW

(SHOWING #4 "S" BARS IN END OF BEAM)

PLAN OF BOX BEAM

EXTERIOR UNIT SHOWN, INTERIOR UNIT SIMILAR EXCEPT OMIT #5 S5 BARS.
FOR LOCATION OF DIAPHRAGMS, SEE "PLAN OF UNIT".
FOR THREADED INSERTS, SEE "THREADED INSERT DETAIL".
FOR REINFORCING STEEL IN DIAPHRAGMS, SEE "DOUBLE DIAPHRAGM DETAILS".

PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

SHEET 3 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE

3′-0″ X 2′-9″ PRESTRESSED CONCRETE BOX BEAM UNIT

60° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-6
1			3			TOTAL SHEETS
2			4			19

PLANS PREPARED BY:
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DRAWN BY: T. BANKOVICH DATE: 8-16
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DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

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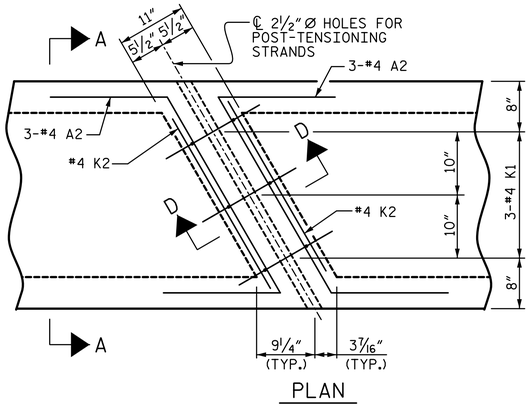
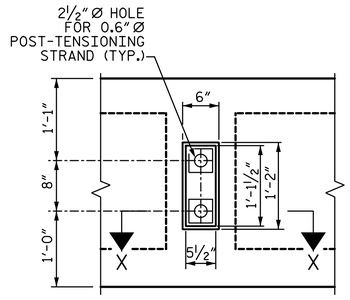
11/1/2016 6:55:25 AM G:\Projects\2015\Division 7 (Hatch Mott)\17BP7R102 (Rockingham 17) (60 33BB 2BMR)\Structures\Drawings\Final\401_17BP7R102_SMU_SUPER.dgn

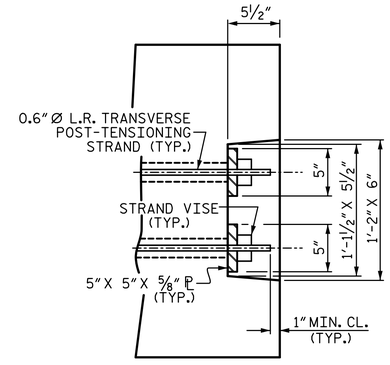


PLAN

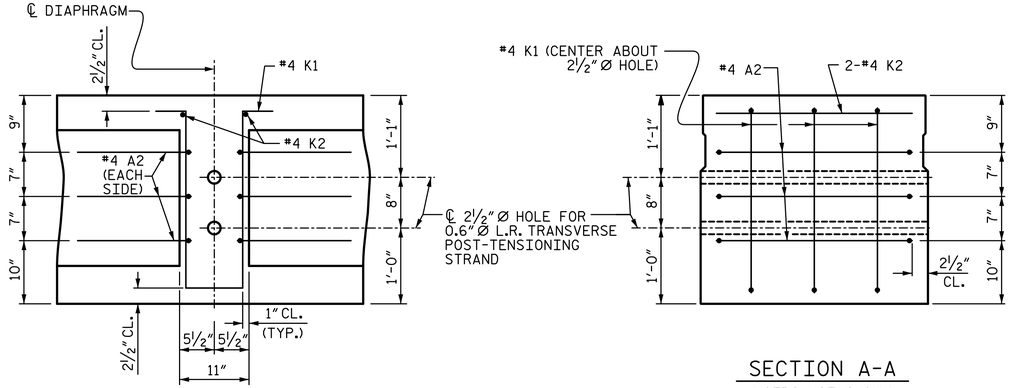


VIEW Y-Y

SHOWING ELEVATION VIEW OF GROUTED RECESS



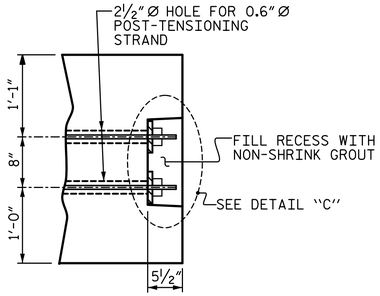
DETAIL "C"



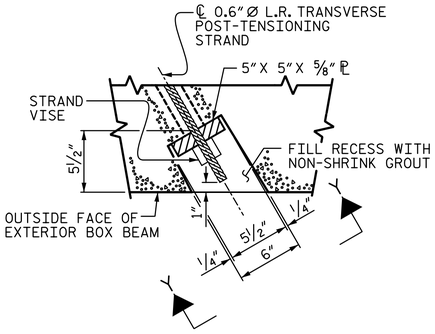
SECTION D-D

SECTION A-A

VOIDS NOT SHOWN



PART SECTION AT RECESS



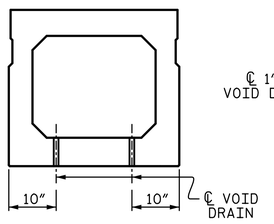
SECTION X-X

SHOWING PLAN VIEW OF GROUTED RECESS

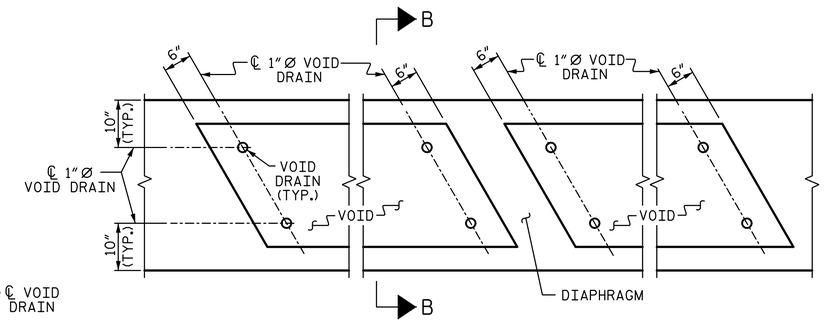
GROUTED RECESS DETAIL AT END OF POST-TENSIONED STRANDS OF EXTERIOR BOX BEAM

DOUBLE DIAPHRAGM DETAILS

#4 "S" BARS NOT SHOWN. #4 "S" BARS MAY BE SHIFTED SLIGHTLY TO CLEAR 2½" Ø HOLE.



SECTION B-B



PART PLAN

VOID DRAIN DETAILS

(DIMENSIONS SHOWN ARE TYPICAL FOR EACH VOID)

DEAD LOAD DEFLECTION AND CAMBER	
	3'-0" x 2'-9"
85' BOX BEAM UNIT	0.6"Ø L.R. STRAND
CAMBER (SLAB ALONE IN PLACE)	2¾" ↑
DEFLECTION DUE TO SUPERIMPOSED DEAD LOAD**	¾" ↓
FINAL CAMBER	2" ↑

** INCLUDES FUTURE WEARING SURFACE

BOX BEAM UNITS REQUIRED			
	NUMBER	LENGTH	TOTAL LENGTH
EXTERIOR B.B.	2	85'-0"	170'-0"
INTERIOR B.B.	9	85'-0"	765'-0"
TOTAL	11	———	935'-0"

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 4 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUPERSTRUCTURE

3'-0" X 2'-9" PRESTRESSED CONCRETE BOX BEAM UNIT

90° SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-7
1			3			TOTAL SHEETS
2			4			19

PLANS PREPARED BY:

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NORTH CAROLINA PROFESSIONAL SEAL 040268 ENGINEER BETSY S. COX
11/1/2016

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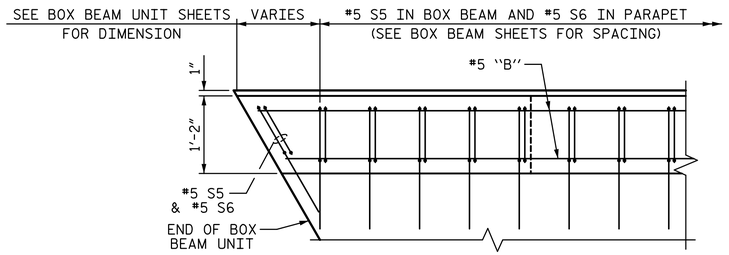
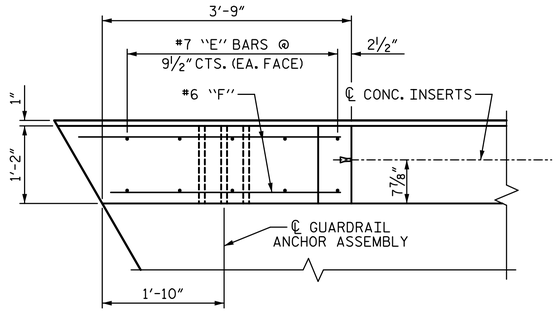
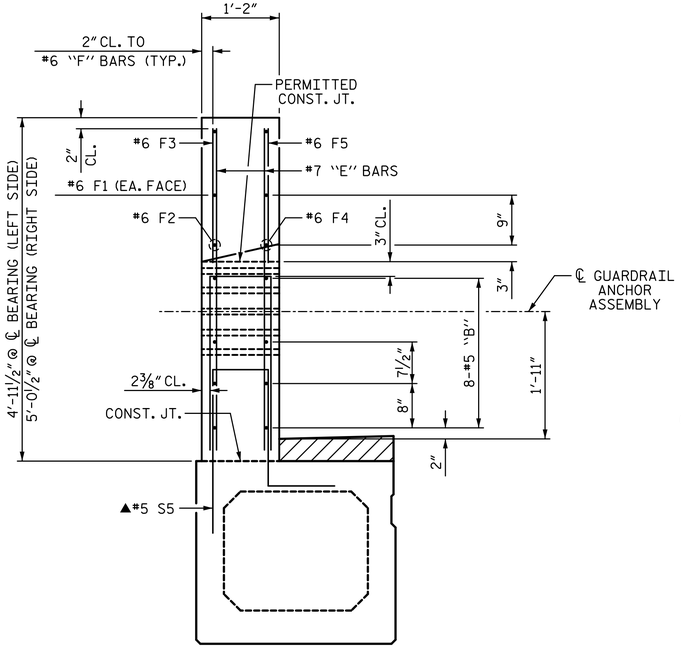
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

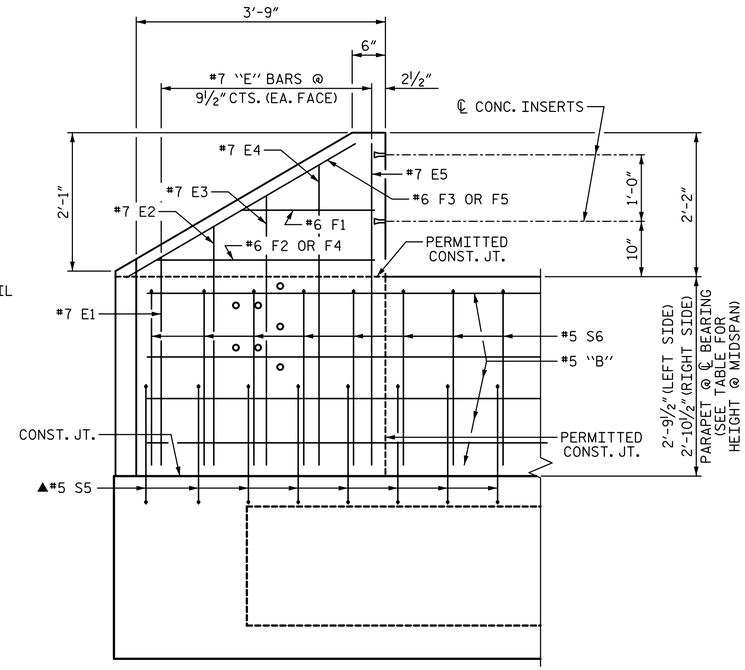




PLAN OF PARAPET

PLAN OF END POST





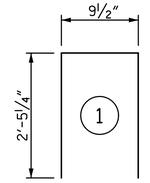
END VIEW

ELEVATION

PARAPET AND END POST FOR TWO BAR RAIL

▲#5 S5 BARS ARE INCLUDED IN BILL OF MATERIAL FOR BOX BEAM

BAR TYPES



BILL OF MATERIAL

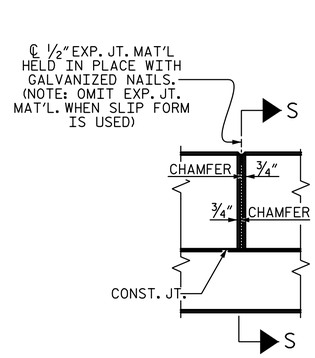
PARAPET AND END POSTS

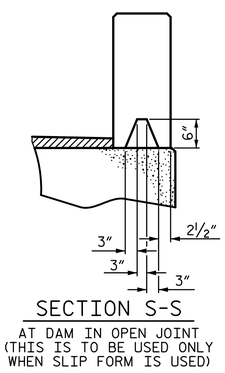
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
✱B9	96	#5	STR	16'-1"	1610
✱E1	8	#7	STR	2'-11"	48
✱E2	8	#7	STR	3'-5"	56
✱E3	8	#7	STR	3'-10"	63
✱E4	8	#7	STR	4'-4"	71
✱E5	8	#7	STR	4'-9"	78
✱F1	8	#6	STR	1'-11"	23
✱F2	4	#6	STR	3'-5"	21
✱F3	4	#6	STR	4'-0"	24
✱F4	4	#6	STR	3'-1"	19
✱F5	4	#6	STR	3'-8"	22
✱S6	226	#5	1	5'-8"	1336

✱EPOXY COATED REINFORCING STEEL	3371 LB
CLASS AA CONCRETE	21.6 CY
1'-2"X 2'-9½" CONCRETE PARAPET	85.0 LF
1'-2"X 2'-10½" CONCRETE PARAPET	85.0 LF

GUTTERLINE ASPHALT THICKNESS & PARAPET HEIGHT

	ASPHALT OVERLAY THICKNESS @ MID-SPAN		PARAPET HEIGHT @ MID-SPAN	
	LEFT	RIGHT	LEFT	RIGHT
85' UNITS	1½"	2½"	2'-7½"	2'-8½"





SECTION S-S

AT DAM IN OPEN JOINT (THIS IS TO BE USED ONLY WHEN SLIP FORM IS USED)

ELEVATION AT EXPANSION JOINTS

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE

CONCRETE PARAPET DETAILS FOR 2 BAR METAL RAIL

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-8
1			3			TOTAL SHEETS
2			4			19

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11/1/2016

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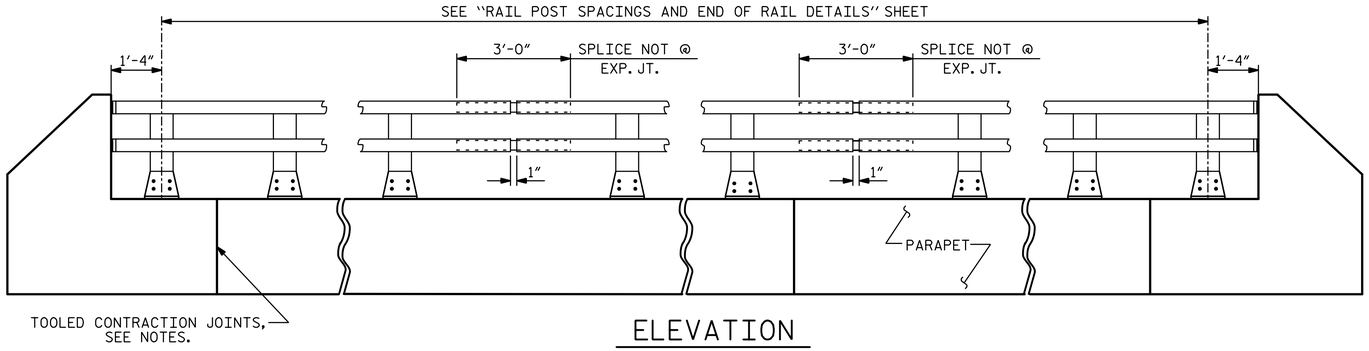
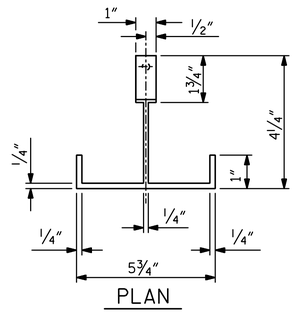
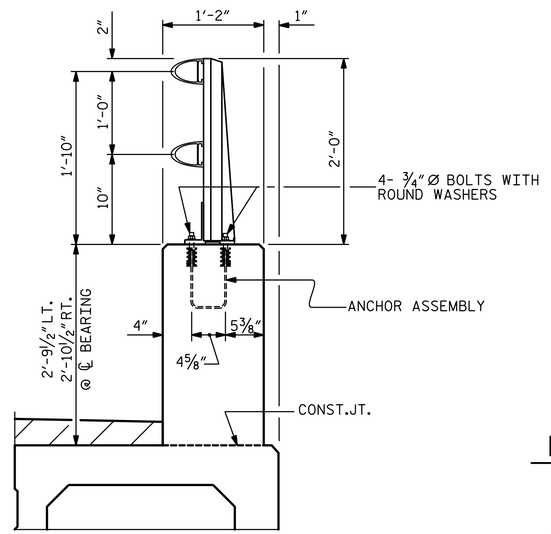


ELEVATION

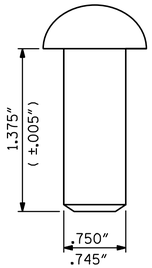
NOTE : FOR ATTACHMENT OF METAL RAIL TO END POST, SEE "RAIL POST SPACING AND END OF RAIL DETAILS" SHEET.



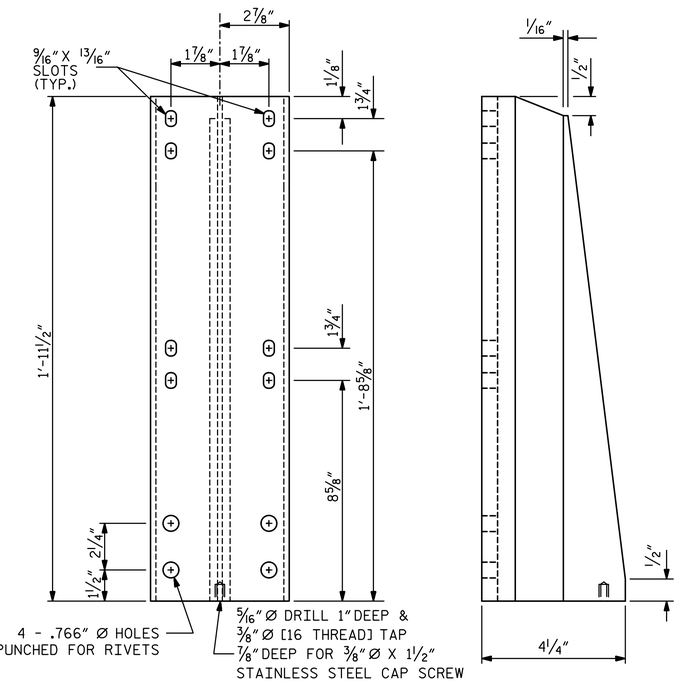
PLAN



SECTION THRU PARAPET AND RAIL



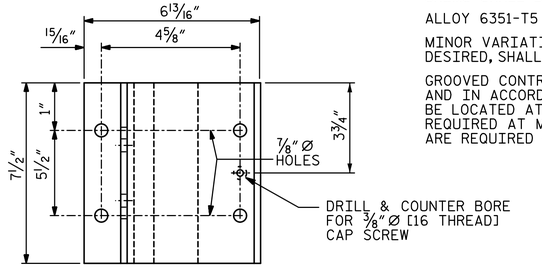
RIVET DETAIL



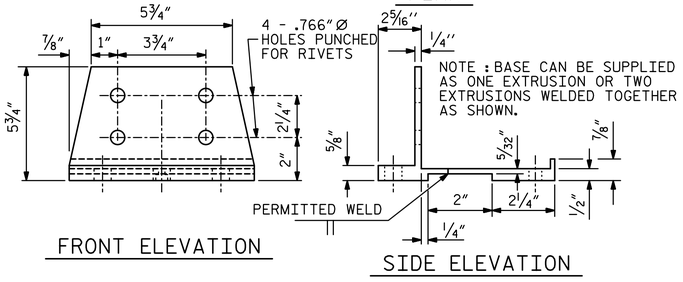
FRONT ELEVATION

SIDE ELEVATION

DETAILS OF POST



PLAN



FRONT ELEVATION

SIDE ELEVATION

POST BASE DETAILS

NOTES:

AT THE CONTRACTOR'S OPTION, METAL RAIL MAY BE EITHER ALUMINUM OR GALVANIZED STEEL IN ACCORDANCE WITH THE REQUIREMENTS OF THE GENERAL NOTES AND THE FOLLOWING SPECIFICATIONS FOR THE ALTERNATE MATERIALS; HOWEVER, THE CONTRACTOR WILL BE REQUIRED TO USE THE SAME RAIL MATERIAL ON ALL STRUCTURES ON THE PROJECT FOR WHICH METAL RAIL IS DESIGNATED.

UNLESS OTHERWISE REQUIRED IN THE CONTRACT DOCUMENTS, THE CONTRACTOR HAS THE OPTION TO USE AN ALTERNATE TO THE 2 BAR METAL RAIL. THE ALTERNATE RAIL SHALL MEET THE REQUIREMENTS OF THE AASHTO LRFDBRIDGE DESIGN SPECIFICATIONS AND MUST BE LISTED ON THE DEPARTMENT'S APPROVED PRODUCTS LIST (APL) UNDER "2 BAR METAL RAIL ALTERNATE". ADJUSTMENTS TO THE CONCRETE PARAPET WILL NOT BE ALLOWED.

ALUMINUM RAILS:

MATERIAL FOR POSTS, BASES AND RAILS, EXPANSION BARS AND CLAMP BARS SHALL BE ASTM B-221 ALLOY 6061-T6.

MATERIAL FOR RIVETS SHALL BE ASTM B316 ALLOY 6061-T6. RIVETS SHALL BE STANDARD BUTTON HEAD AND CONE POINT COLD DRIVEN AS PER DRAWING.

THE BASE OF RAIL POSTS, OR ANY OTHER ALUMINUM SURFACE IN CONTACT WITH CONCRETE SHALL BE THOROUGHLY COATED WITH AN ALUMINUM IMPREGNATED CAULKING COMPOUND OF APPROVED QUALITY.

MATERIAL FOR SHIMS TO BE ASTM B209 ALLOY 6061-T6.

GALVANIZED STEEL RAILS:

MATERIAL AND GALVANIZING ARE TO CONFORM TO THE FOLLOWING SPECIFICATIONS:

POST, POST BASES, RAILS, EXPANSION BARS AND CLAMP BARS: AASHTO M270 GRADE 36 STRUCTURAL STEEL - GALVANIZED TO AASHTO M111.

RIVETS: RIVETS SHALL MEET THE REQUIREMENTS OF ASTM A502 FOR GRADE 1 RIVETS.

THE CUT ENDS OF GALVANIZED STEEL RAILING, AFTER GRINDING SMOOTH SHALL BE GIVEN TWO COATS OF ZINC RICH PAINT MEETING THE REQUIREMENTS OF FEDERAL SPECIFICATION MIL-P-26915 USAF TYPE 1, OR OF FEDERAL SPECIFICATIONS TT-P-641.

SHIMS: SHIMS SHALL MEET THE REQUIREMENTS OF ASTM A570 FOR GRADE 33 OR A611 FOR GRADE C AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M111.

RAIL CAPS: RAIL CAPS SHALL MEET THE REQUIREMENTS OF ASTM A570 FOR GRADE 33 OR A611 FOR GRADE C AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M111.

GENERAL NOTES:

RAILING SHALL BE CONTINUOUS FROM END POST TO END POST OF BRIDGE. EACH JOINT IN RAIL LENGTH SHALL BE SPLICED AS DETAILED. PANEL LENGTHS OF RAIL SHALL BE ATTACHED TO A MINIMUM OF THREE POSTS.

FOR END OF RAIL TO CLEAR FACE OF CONCRETE END POST DIMENSION, SEE "RAIL POST SPACING AND END OF RAIL DETAILS" SHEET.

CAP SCREWS SHALL BE ASTM F593 ALLOY 305 STAINLESS STEEL. WASHERS SHALL MEET THE REQUIREMENTS OF ASTM F844 EXCEPT THEY SHALL BE MADE FROM ALLOY 304 STAINLESS STEEL.

CERTIFIED MILL REPORTS ARE REQUIRED FOR RAILS AND POSTS. SHOP INSPECTION IS NOT REQUIRED.

METAL RAIL POSTS SHALL BE SET NORMAL TO CURB GRADE.

METHOD OF MEASUREMENT FOR METAL RAILS: FOR LENGTH OF METAL RAILS TO BE PAID FOR, SEE THE STANDARD SPECIFICATIONS.

CURVED RAIL USAGE: WHERE RAILS ARE TO BE USED ON BRIDGES ON HORIZONTAL AND/OR VERTICAL CURVATURE THE CONTRACTOR MAY, AT HIS OPTION, HAVE THE REQUIRED CURVATURE IN THE RAIL FORMED IN THE SHOP OR IN THE FIELD. IN EITHER EVENT, THE RAIL SHALL CONFORM WITHOUT BUCKLING OR KINKING TO THE REQUIRED CURVATURE IN A UNIFORM MANNER ACCEPTABLE TO THE ENGINEER.

TO INSURE FUTURE IDENTIFICATION OF THE FABRICATOR, A PERMANENT IDENTIFYING MARK SHALL BE PLACED ON EACH POST. THE METHOD OF MARKING AND LOCATION SHALL BE SUCH THAT IT DOES NOT DETRACT FROM THE APPEARANCE OF THE POST, BUT REMAINS VISIBLE AFTER RAIL PLACEMENT.

SHIMS SHALL BE USED AS NECESSARY FOR POST ALIGNMENT.

ALLOY 6351-T5 MAY BE SUBSTITUTED FOR ALLOY 6061-T6 WHERE APPLICABLE.

MINOR VARIATIONS IN DETAILS OF METAL RAIL WILL BE CONSIDERED. DETAILS OF SUCH VARIATIONS, IF DESIRED, SHALL BE SUBMITTED FOR APPROVAL.

GROOVED CONTRACTION JOINTS, 1/2" IN DEPTH, SHALL BE TOOLED IN ALL EXPOSED FACES OF THE PARAPET AND IN ACCORDANCE WITH ARTICLE 825-10(B) OF THE STANDARD SPECIFICATIONS. A CONTRACTION JOINT SHALL BE LOCATED AT EACH THIRD POINT BETWEEN PARAPET EXPANSION JOINTS. ONLY ONE CONTRACTION JOINT IS REQUIRED AT MIDPOINT OF PARAPET SEGMENTS LESS THAN 20 FEET IN LENGTH AND NO CONTRACTION JOINTS ARE REQUIRED FOR THOSE SEGMENTS LESS THAN 10 FEET IN LENGTH.

PAY LENGTH = 153.65 LF

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 1 OF 2

DRAWN BY: T. BANKOVICH DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

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11/1/2016

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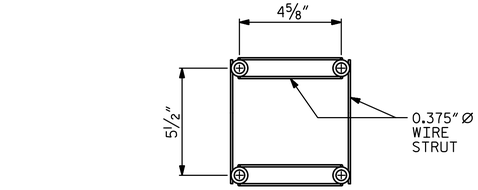
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE

2 BAR METAL RAIL

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-9
1			3			TOTAL SHEETS
2			4			19



STRUCTURAL CONCRETE ANCHOR ASSEMBLY NOTES:

THE STRUCTURAL CONCRETE ANCHOR ASSEMBLY SHALL CONSIST OF THE FOLLOWING COMPONENTS :

A. FERRULES SHALL BE MADE FROM STEEL MEETING THE REQUIREMENTS OF AASHTO M169, GRADE 12L14 AND SHALL HAVE A MINIMUM LENGTH OF THREADS OF 2" FOR ¾" FERRULES.

B. 4 - ¾" Ø X 2½" BOLTS WITH WASHERS. BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307. BOLTS AND WASHERS SHALL BE GALVANIZED. AT THE CONTRACTOR'S OPTION, STAINLESS STEEL BOLTS AND WASHERS MAY BE USED AS AN ALTERNATE FOR THE ¾" Ø X 2½" GALVANIZED BOLTS AND WASHERS. THEY SHALL CONFORM TO OR EXCEED THE MECHANICAL REQUIREMENTS OF ASTM A307. THE USE OF THIS ALTERNATE SHALL BE APPROVED BY THE ENGINEER.

C. WIRE STRUT SHOWN IN THE CONCRETE ANCHOR ASSEMBLY DETAIL IS THE MINIMUM ALLOWABLE SIZE AND SHALL HAVE A MINIMUM TENSILE STRENGTH OF 100,000 PSI. AS AN OPTION, A 7/16" Ø WIRE STRUT WITH A MINIMUM TENSILE STRENGTH OF 90,000 PSI IS ACCEPTABLE.

D. THE METAL RAIL ANCHOR ASSEMBLIES TO BE HOT DIPPED GALVANIZED TO CONFORM TO REQUIREMENTS OF AASHTO M111.

E. THE COST OF THE METAL RAIL ANCHOR ASSEMBLY WITH BOLTS AND WASHERS COMPLETE IN PLACE SHALL BE INCLUDED IN THE PRICE BID FOR LINEAR FEET OF METAL RAIL.

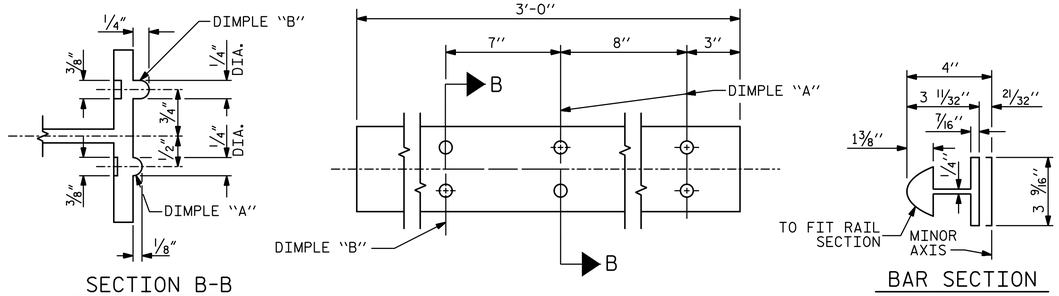
F. BOLTS TO BE TIGHTENED ONE-HALF TURN WITH A WRENCH FROM A FINGER-TIGHT POSITION.

THE CONTRACTOR MAY USE ADHESIVELY ANCHORED ANCHOR BOLTS IN PLACE OF THE METAL RAIL ANCHOR ASSEMBLY. LEVEL ONE FIELD TESTING IS REQUIRED, AND THE YIELD LOAD OF THE ¾" Ø BOLT IS 10 KIPS. FOR ADHESIVELY ANCHORED ANCHOR BOLTS OR DOWELS, SEE THE STANDARD SPECIFICATIONS.

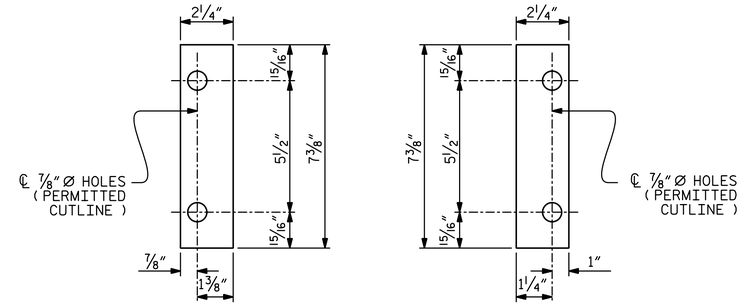
WHEN ADHESIVELY ANCHORED ANCHOR BOLTS ARE USED, BOLTS SHALL MEET THE REQUIREMENTS OF ASTM F593 ALLOY 304 STAINLESS STEEL WITH MINIMUM 75,000 PSI ULTIMATE STRENGTH. NUTS SHALL MEET THE REQUIREMENTS OF ASTM F594 ALLOY 304 STAINLESS STEEL AND WASHERS SHALL MEET THE REQUIREMENTS OF ASTM F844 EXCEPT THEY SHALL BE MADE FROM ALLOY 304 STAINLESS STEEL.

4-BOLT METAL RAIL ANCHOR ASSEMBLY

(34 ASSEMBLIES REQUIRED)

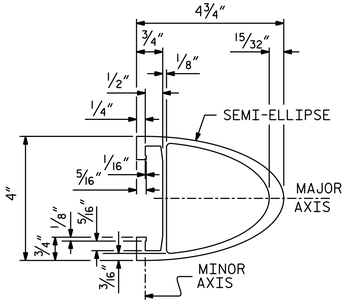


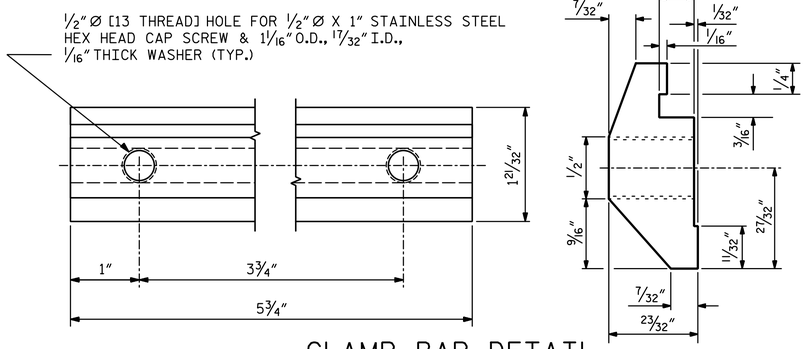
EXPANSION BAR DETAILS



SHIM DETAILS

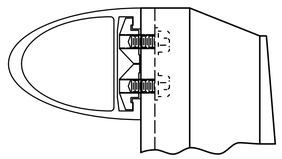
NOTE :
SHIMS MAY BE CUT ALONG PERMITTED CUTLINE OR SLOTTED TO EDGE OF PLATE TO FACILITATE PLACEMENT.



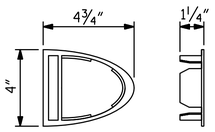


CLAMP BAR DETAIL

(4 REQUIRED PER POST)



CLAMP ASSEMBLY



RAIL CAP

PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

SHEET 2 OF 2

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
SUPERSTRUCTURE

2 BAR METAL RAIL

DRAWN BY: T. BANKOVICH DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

PLANS PREPARED BY:

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11/1/2016

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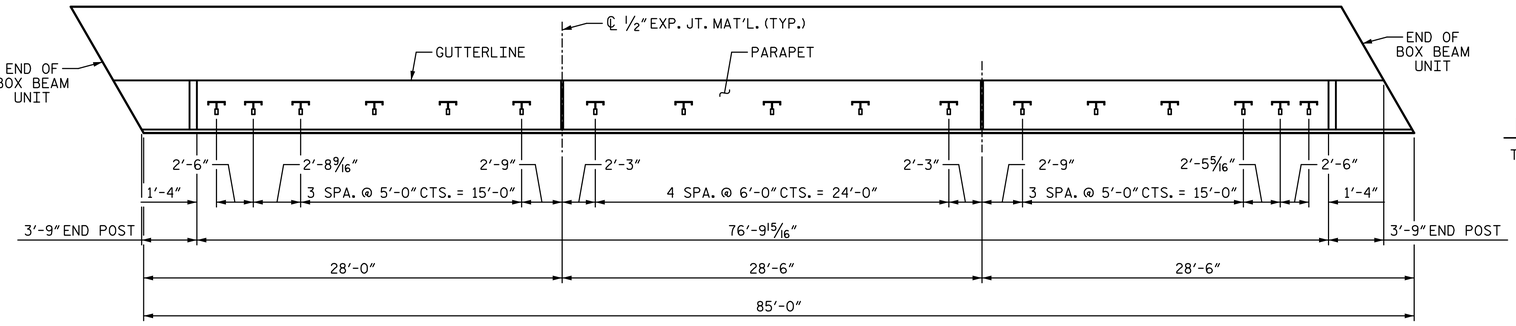


PLAN OF RAIL POST SPACING

(RIGHT EXTERIOR UNIT SHOWN, LEFT EXTERIOR UNIT SIMILAR)

STRUCTURAL CONCRETE INSERT NOTES:

THE STRUCTURAL CONCRETE INSERT ASSEMBLY SHALL CONSIST OF THE FOLLOWING COMPONENTS:

A. FERRULES SHALL BE MADE FROM STEEL MEETING THE REQUIREMENTS OF AASHTO M169, GRADE 12L14 AND SHALL HAVE A MINIMUM LENGTH OF THREADS OF 1½''.

B. 1 - ¾'' Ø X 1⅝'' BOLT WITH WASHER. BOLT SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307. BOLT AND WASHER SHALL BE GALVANIZED. (AT THE CONTRACTOR'S OPTION, STAINLESS STEEL BOLT AND WASHER MAY BE USED AS AN ALTERNATE FOR THE ¾'' Ø X 1⅝'' GALVANIZED BOLT AND WASHER. THEY SHALL CONFORM TO OR EXCEED THE MECHANICAL REQUIREMENTS OF ASTM A307. THE USE OF THIS ALTERNATE SHALL BE APPROVED BY THE ENGINEER.)

WIRE STRUT SHOWN IN THE CONCRETE INSERT ASSEMBLY DETAIL IS THE MINIMUM ALLOWABLE SIZE AND SHALL HAVE A MINIMUM TENSILE STRENGTH OF 100,000 PSI. AS AN OPTION, A 7/16'' Ø WIRE STRUT WITH A MINIMUM TENSILE STRENGTH OF 90,000 PSI IS ACCEPTABLE.

METAL RAIL TO END POST CONNECTION NOTES:

THE METAL RAIL TO END POST CONNECTION SHALL CONSIST OF THE FOLLOWING COMPONENTS:

A. ½'' PLATES SHALL CONFORM TO AASHTO M270 GRADE 36 AND SHALL BE GALVANIZED AFTER FABRICATION.

B. ¾'' STRUCTURAL CONCRETE INSERT SHALL HAVE A WORKING LOAD SHEAR CAPACITY OF 4800 LBS. THE FERRULES SHALL ENGAGE A ¾''Ø X 1⅝'' BOLT WITH 2'' O.D. WASHER IN PLACE. THE ¾''Ø X 1⅝'' BOLT SHALL HAVE N. C. THREADS.

C. CAP SCREWS FOR RAIL ATTACHMENT TO ANGLE SHALL CONFORM TO THE REQUIREMENTS OF ASTM F593 ALLOY 305 STAINLESS STEEL. CAP SCREWS TO BE CENTERED IN SLOTS AT 60°F.

D. STANDARD CLAMP BARS (SEE METAL RAIL SHEET).

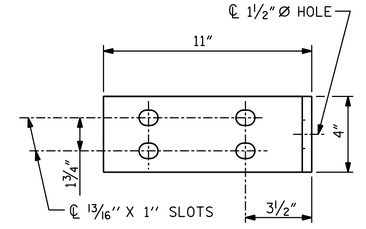
E. ½'' Ø PIPE SLEEVES (IF REQUIRED) TO BE GALVANIZED.

THE COST OF THE STANDARD CLAMP BARS AND CAP SCREWS USED IN THE METAL RAIL TO END POST CONNECTION SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR LINEAR FEET OF 1 OR 2 BAR METAL RAILS.

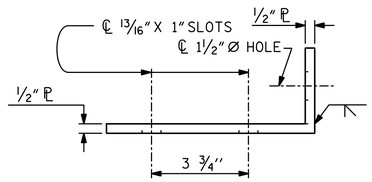
THE ¾'' STRUCTURAL CONCRETE INSERT WITH BOLT SHALL BE ASSEMBLED IN THE SHOP.

THE COST OF THE ¾'' STRUCTURAL CONCRETE INSERT ASSEMBLY, AND THE ½'' PLATES COMPLETE IN PLACE SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS.

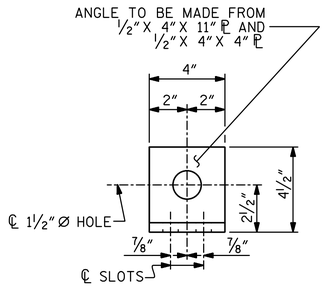
THE CONTRACTOR, AT HIS OPTION, MAY USE AN ADHESIVE BONDING SYSTEM IN LIEU OF THE STRUCTURAL CONCRETE INSERT EMBEDDED IN THE END POST. IF THE ADHESIVE BONDING SYSTEM IS USED, THE ¾'' Ø X 1⅝'' BOLT WITH WASHER SHALL BE REPLACED WITH A ¾''Ø X 6½'' BOLT AND 2'' O.D. WASHER. ALL SPECIFICATIONS THAT APPLY TO THE ¾'' Ø X 1⅝'' BOLT SHALL APPLY TO THE ¾''Ø X 6 ½'' BOLT. FIELD TESTING OF THE ADHESIVE BONDING SYSTEM IS NOT REQUIRED.



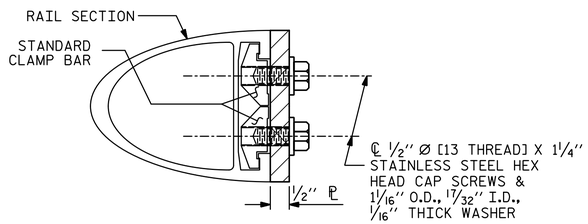
ELEVATION



TOP VIEW

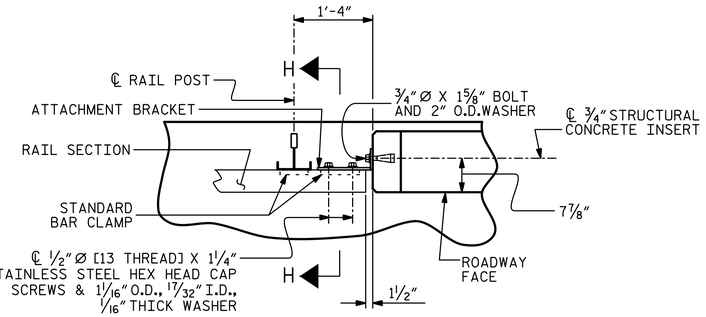


END VIEW

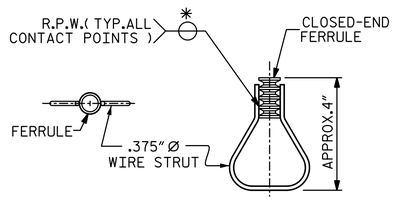


SECTION H-H

DETAILS FOR ATTACHING METAL RAILS TO END POST



PLAN - RAIL AND END POST



PLAN ELEVATION

STRUCTURAL CONCRETE INSERT

✳EACH WELDED ATTACHMENT OF WIRE TO FERRULE SHALL DEVELOP THE TENSILE STRENGTH OF THE WIRE.

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

DRAWN BY: T. BANKOVICH	DATE: 8-16				
CHECKED BY: B.S. COX	DATE: 8-16				
DESIGN ENGINEER OF RECORD: B.S. COX	DATE: 8-16				

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11/1/2016

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STATE OF NORTH CAROLINA
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RAIL POST SPACING AND END OF RAIL DETAILS

FOR TWO BAR METAL RAILS

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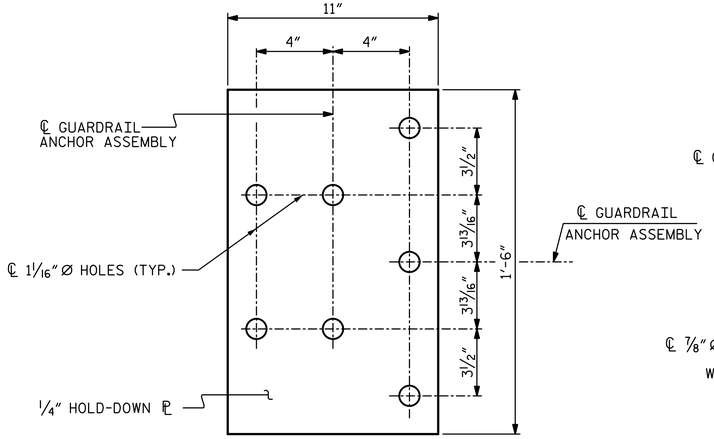
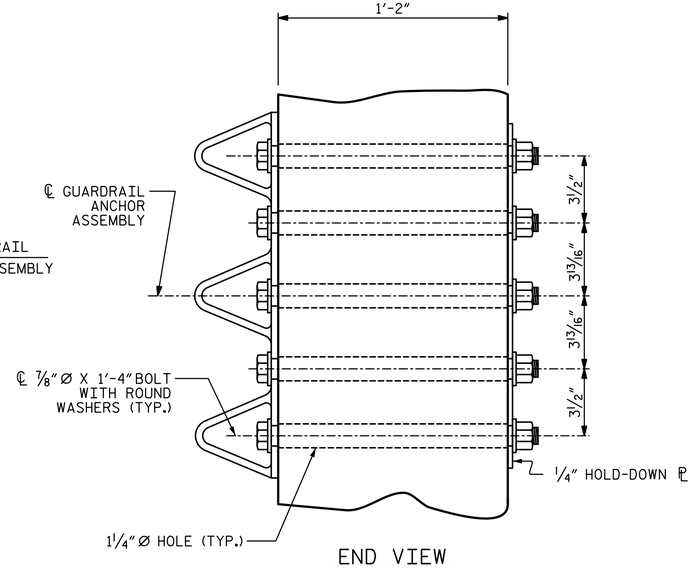
1			3			TOTAL SHEETS
2			4			19



PLAN



END VIEW

GUARDRAIL ANCHOR ASSEMBLY DETAILS

NOTES:

THE GUARDRAIL ANCHOR ASSEMBLY SHALL CONSIST OF A 1/4'' HOLD DOWN PLATE AND 7 - 7/8'' Ø BOLTS WITH NUTS AND WASHERS.

THE HOLD-DOWN PLATE SHALL CONFORM TO AASHTO M270 GRADE 36. AFTER FABRICATION, THE HOLD-DOWN PLATE SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111.

BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307 AND NUTS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M291. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED. AT THE CONTRACTOR'S OPTION, STAINLESS STEEL BOLTS, NUTS AND WASHERS MAY BE USED AS AN ALTERNATE FOR THE 7/8'' Ø GALVANIZED BOLTS, NUTS AND WASHERS. THEY SHALL CONFORM TO OR EXCEED THE MECHANICAL REQUIREMENTS OF ASTM A307. THE USE OF THIS ALTERNATE SHALL BE APPROVED BY THE ENGINEER.

THE GUARDRAIL ANCHOR ASSEMBLY IS REQUIRED AT ALL POINTS WHERE APPROACH GUARDRAIL IS TO BE ATTACHED TO THE END OF THE PARAPET. FOR POINTS OF ATTACHMENT, SEE SKETCH.

AFTER INSTALLATION, THE EXPOSED THREAD OF THE BOLT SHALL BE BURRED WITH A SHARP POINTED TOOL.

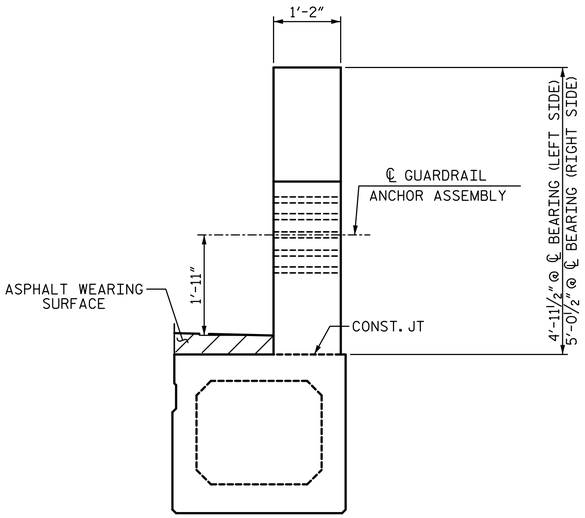
THE COST OF THE GUARDRAIL ANCHOR ASSEMBLIES WITH BOLTS, NUTS AND WASHERS COMPLETE IN PLACE, SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS.

THE VERTICAL REINFORCING BARS MAY BE SHIFTED SLIGHTLY IN THE END POST TO CLEAR ASSEMBLY BOLTS.

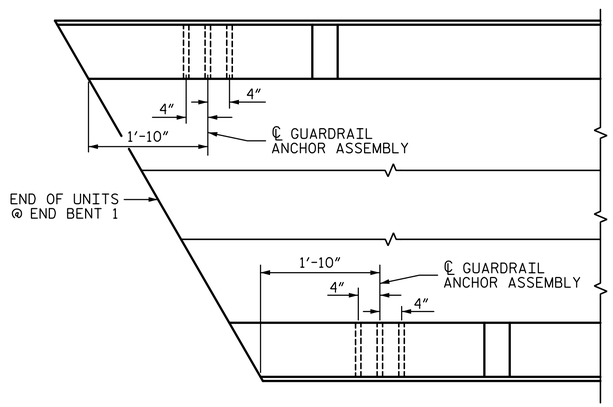
THE 1 1/4'' Ø HOLES SHALL BE FORMED OR DRILLED WITH A CORE BIT. IMPACT TOOLS WILL NOT BE PERMITTED. ANY CONCRETE DAMAGED BY THIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.

SKETCH SHOWING POINTS OF ATTACHMENT

✳ LOCATION OF GUARDRAIL ATTACHMENT



END VIEW



PLAN

(END BENT 1 SHOWN, END BENT 2 SIMILAR)

LOCATION OF GUARDRAIL ANCHOR AT END POST

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

STATE OF NORTH CAROLINA
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RALEIGH

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GUARDRAIL ANCHORAGE DETAILS FOR METAL RAILS

DRAWN BY:	T. BANKOVICH	DATE:	8-16
CHECKED BY:	B.S. COX	DATE:	8-16
DESIGN ENGINEER OF RECORD:	B.S. COX	DATE:	8-16

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11/1/2016

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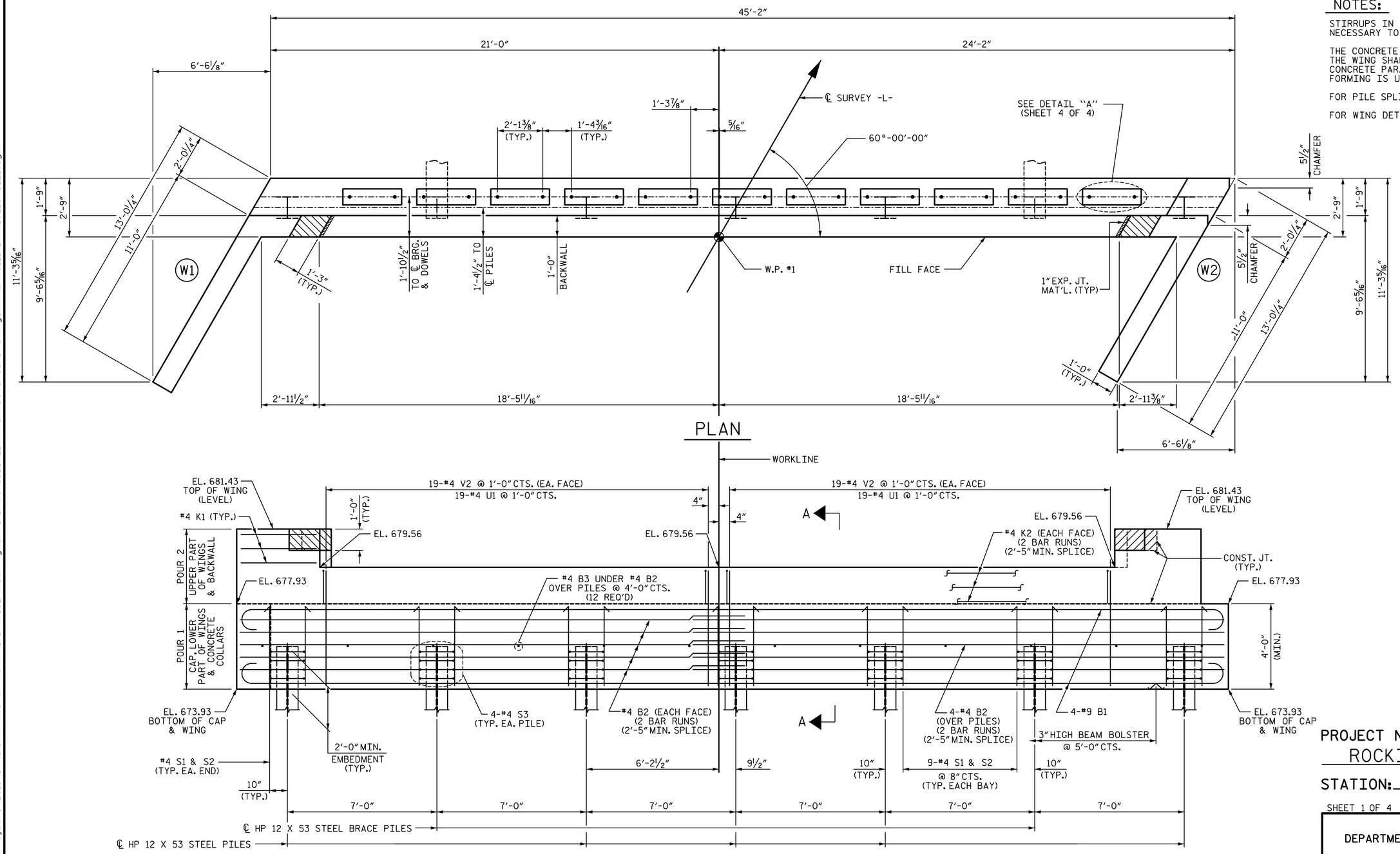
NOTES:

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR DOWELS.

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE CONCRETE PARAPET IS CAST IF SLIP FORMING IS USED.

FOR PILE SPLICE DETAILS, SEE SHEET 4 OF 4.

FOR WING DETAILS, SEE SHEET 3 OF 4.



PLAN

ELEVATION

WINGS NOT SHOWN FOR CLARITY.
FOR SECTION A-A, SEE SHEET 4 OF 4.
CONCRETE COLLARS FOR STEEL PILES NOT SHOWN IN PLAN AND ELEVATION VIEWS FOR CLARITY.
SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL", SHEET 4 OF 4.

PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

SHEET 1 OF 4

STATE OF NORTH CAROLINA
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END BENT 1

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11/1/2016

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CHECKED BY:	B.S. COX	DATE: 8-16
DESIGN ENGINEER OF RECORD:	B.S. COX	DATE: 8-16

NOTES:

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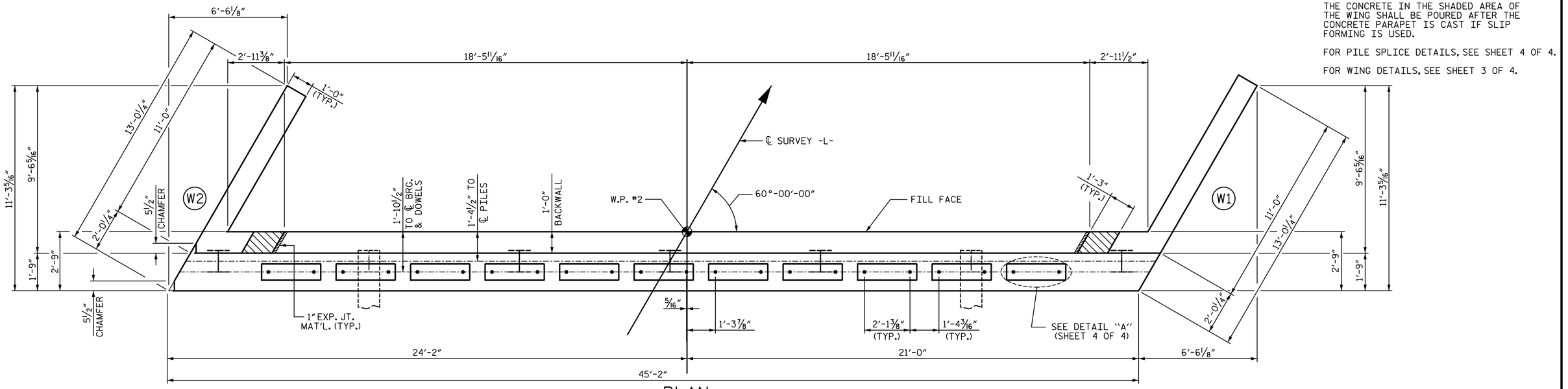
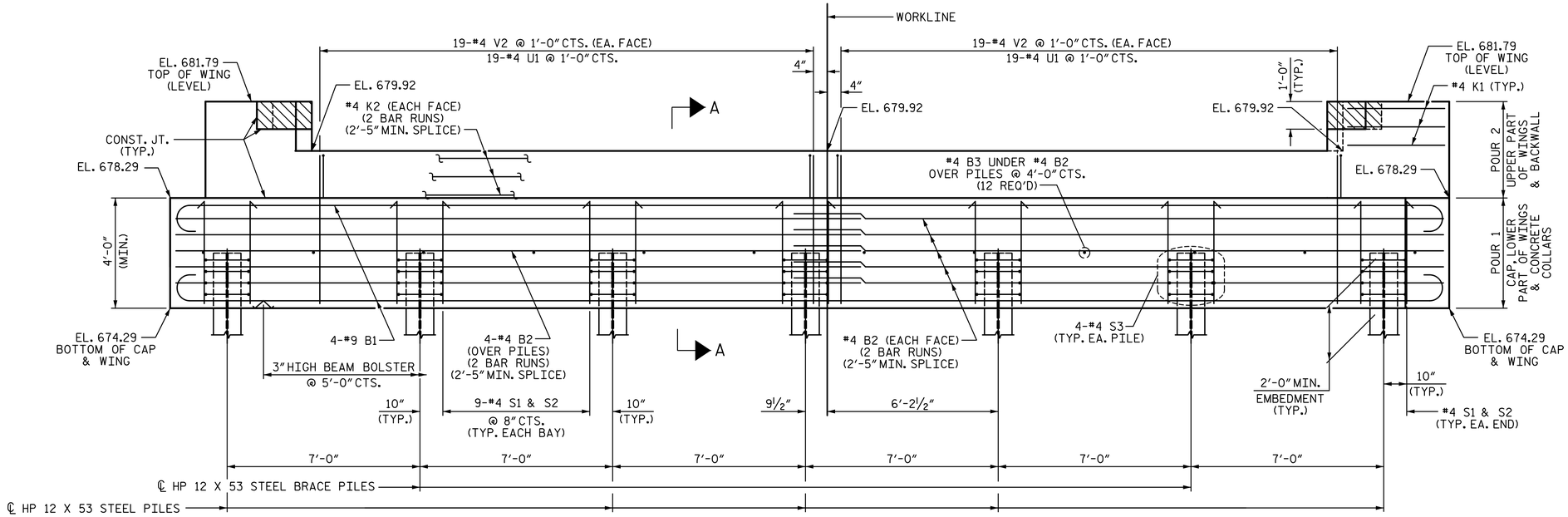
THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE CONCRETE PARAPET IS CAST IF SLIP FORMING IS USED.

FOR PILE SPLICE DETAILS, SEE SHEET 4 OF 4.

FOR WING DETAILS, SEE SHEET 3 OF 4.



PLAN



ELEVATION

WINGS NOT SHOWN FOR CLARITY.
FOR SECTION A-A, SEE SHEET 4 OF 4.
CONCRETE COLLARS FOR STEEL PILES NOT SHOWN IN PLAN AND ELEVATION VIEWS FOR CLARITY.
SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL", SHEET 4 OF 4.

DRAWN BY: S.D. COOPER DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
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ROCKINGHAM COUNTY
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SHEET 2 OF 4

STATE OF NORTH CAROLINA
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RALEIGH
SUBSTRUCTURE

END BENT 2

REVISIONS						SHEET NO.
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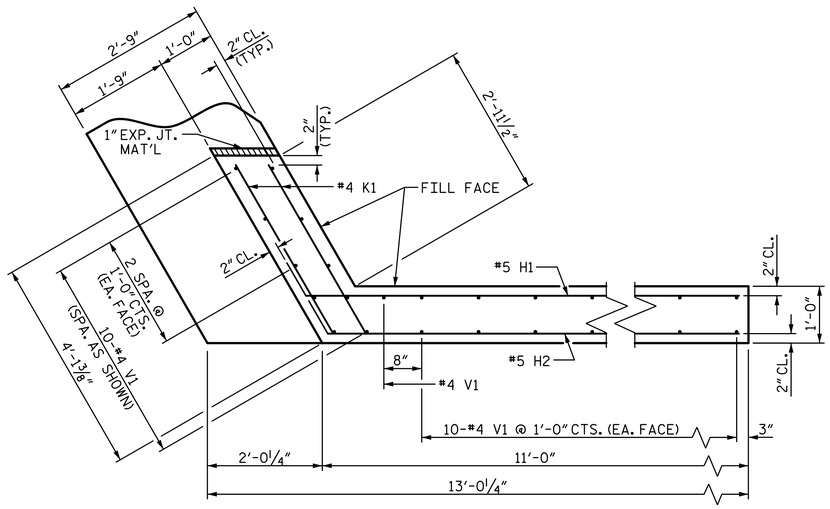
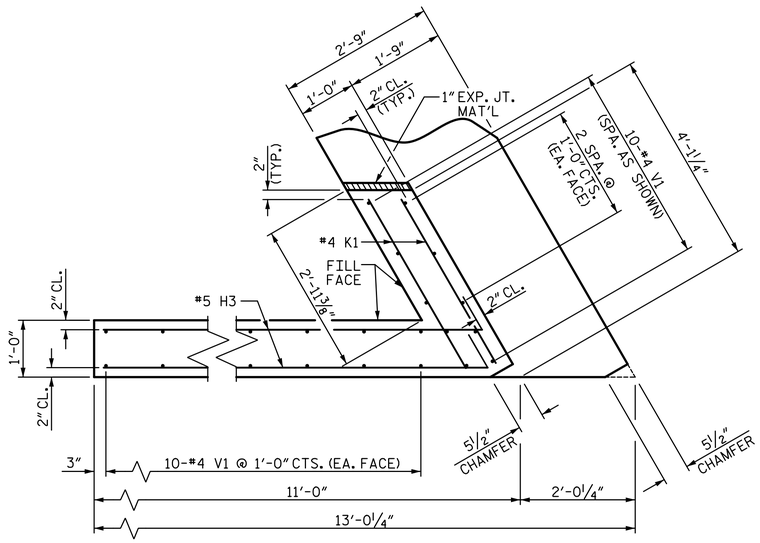
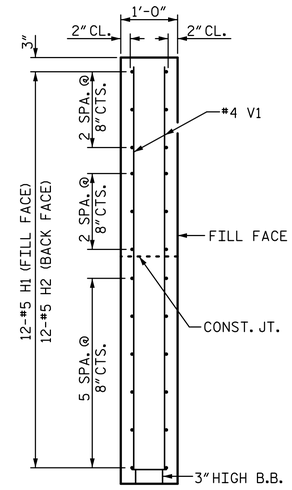
1			3			TOTAL SHEETS
2			4			19



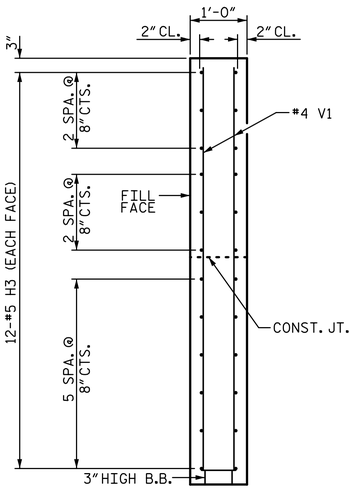
PLAN OF WING (W1)



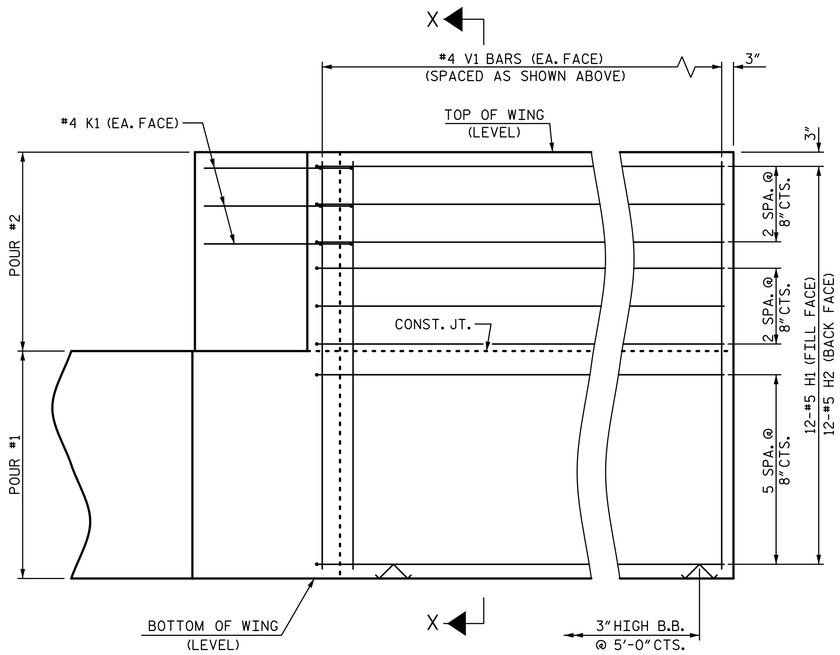
PLAN OF WING (W2)



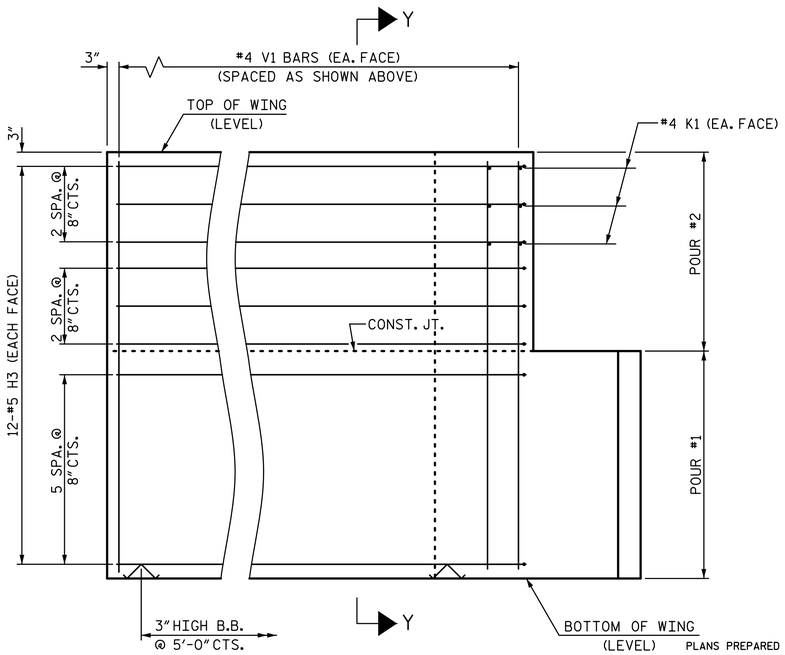
SECTION X-X



SECTION Y-Y



ELEVATION OF WING (W1)



ELEVATION OF WING (W2)

WING DETAILS

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 3 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
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END BENT
WING DETAILS

DRAWN BY: S.D. COOPER	DATE: 8-16		
CHECKED BY: B.S. COX	DATE: 8-16		
DESIGN ENGINEER OF RECORD: B.S. COX	DATE: 8-16		

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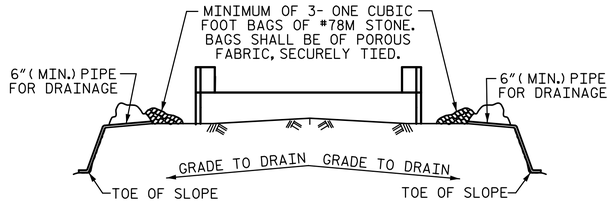
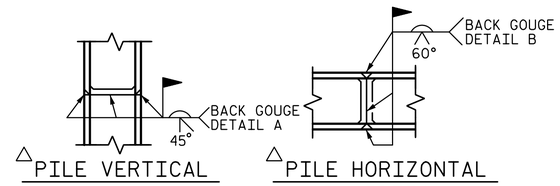


BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY, OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

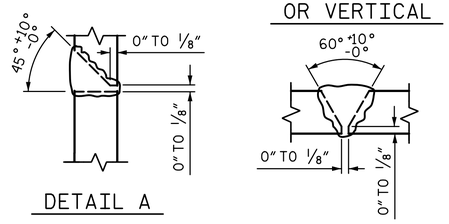
NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT



△PILE VERTICAL

△PILE HORIZONTAL OR VERTICAL



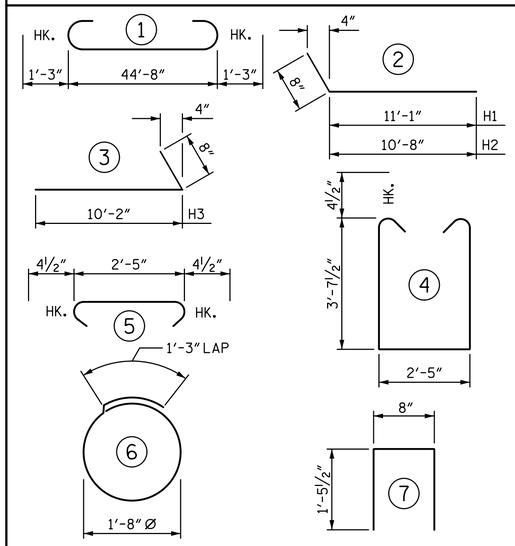
DETAIL A

DETAIL B

△POSITION OF PILE DURING WELDING.

PILE SPLICE DETAILS

BAR TYPES



ALL BAR DIMENSIONS ARE OUT TO OUT.

END BENT 1	
HP 12 X 53 STEEL PILES	
NO: 7	LF = 175
STEEL PILE POINTS	NO: 7

END BENT 2	
HP 12 X 53 STEEL PILES	
NO: 7	LF = 140
STEEL PILE POINTS	NO: 7

PILE EXCAVATION IN SOIL 15 LF

PILE EXCAVATION NOT IN SOIL 10 LF

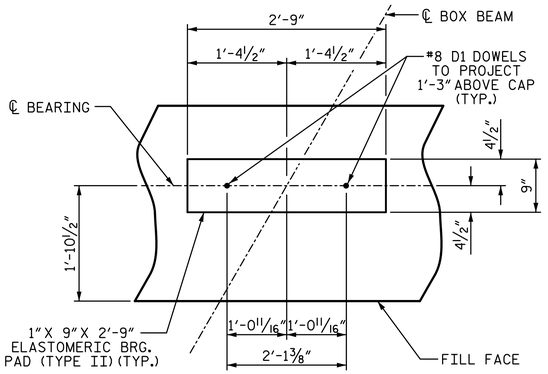
BILL OF MATERIAL
FOR ONE END BENT

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
B1	8	#9	1	47'-2"	1283
B2	28	#4	STR	23'-8"	443
B3	12	#4	STR	2'-5"	19
D1	22	#8	STR	2'-3"	132
H1	12	#5	2	11'-9"	147
H2	12	#5	2	11'-4"	142
H3	24	#5	3	10'-10"	271
K1	12	#4	STR	3'-5"	27
K2	12	#4	STR	23'-8"	190
S1	56	#4	4	10'-5"	390
S2	56	#4	5	3'-2"	118
S3	28	#4	6	6'-6"	122
U1	38	#4	7	3'-7"	91
V1	61	#4	STR	7'-2"	292
V2	76	#4	STR	5'-3"	267

REINFORCING STEEL (FOR ONE END BENT) 3934 LB

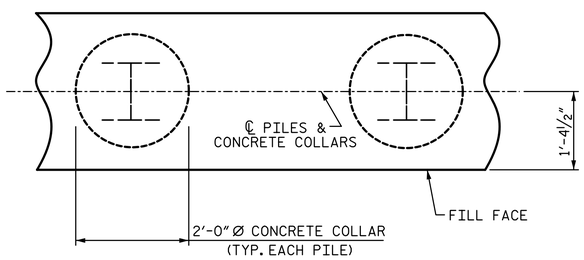
CLASS A CONCRETE BREAKDOWN (FOR ONE END BENT)

POUR #1	CAP, LOWER PART OF WINGS & COLLARS	22.6 CY
POUR #2	BACKWALL & UPPER PART OF WINGS	5.9 CY
TOTAL CLASS A CONCRETE		28.5 CY

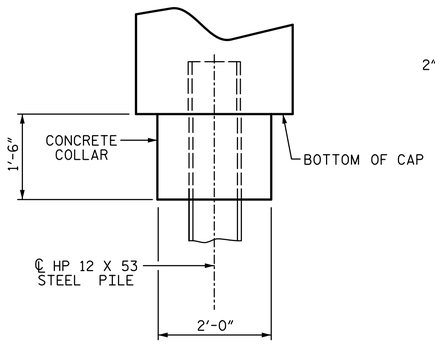


DETAIL "A"

(END BENT 1 SHOWN, END BENT 2 SIMILAR BY ROTATION)



PLAN



ELEVATION

CORROSIION PROTECTION FOR STEEL PILES DETAIL

(END BENT 1 SHOWN, END BENT 2 SIMILAR BY ROTATION)

SECTION A-A

(CONCRETE COLLAR NOT SHOWN FOR CLARITY. SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL.")

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 4 OF 4

STATE OF NORTH CAROLINA
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END BENT 1 & 2 DETAILS

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11/1/2016

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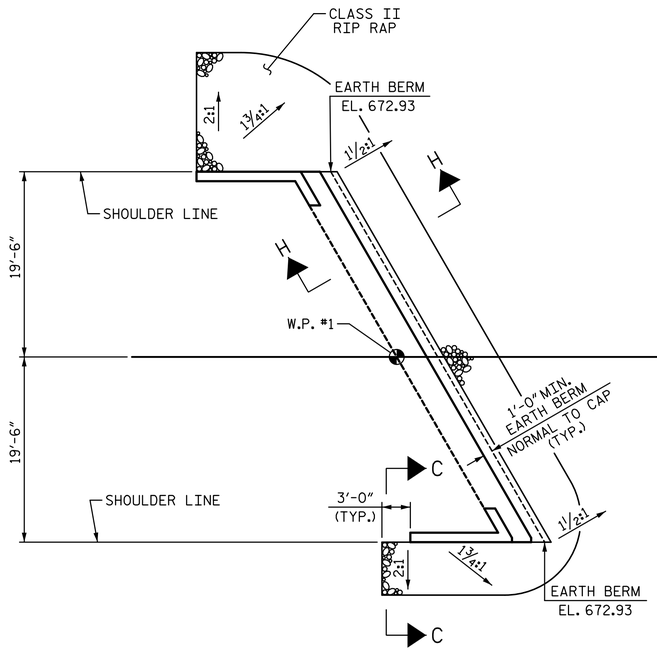
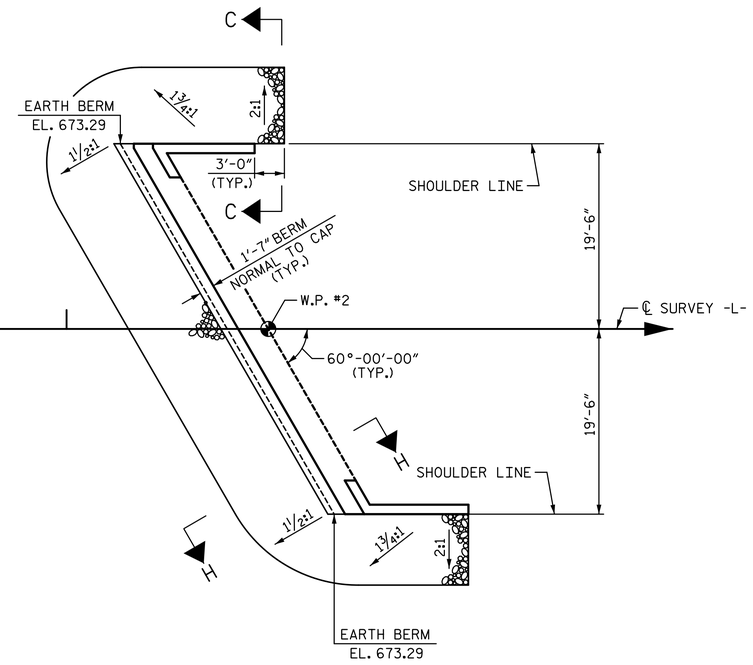


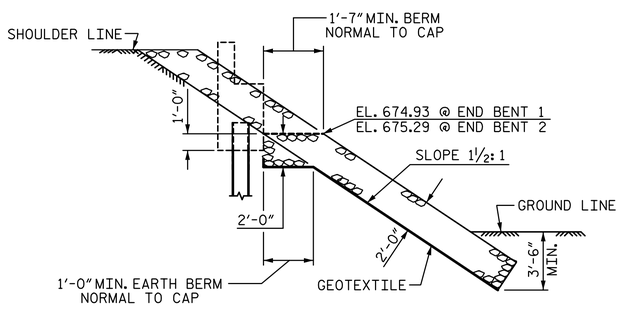
@ END BENT 1



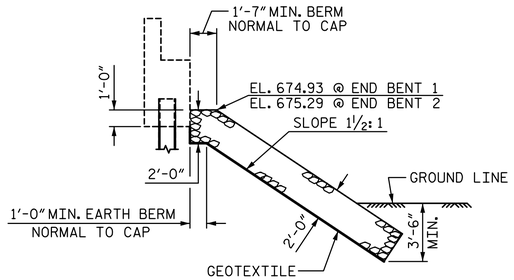
@ END BENT 2

ESTIMATED QUANTITIES		
BRIDGE @ STA. 13+77.50 -L-	RIP RAP CLASS II (2'-0" THICK)	GEOTEXTILE FOR DRAINAGE
	TONS	SQUARE YARDS
END BENT 1	105	120
END BENT 2	130	145

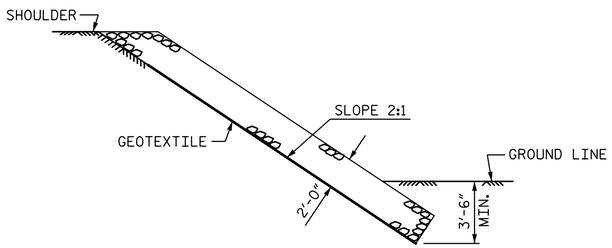
PLAN OF RIP RAP



SECTION H-H



℄ SECTION
BERM RIP RAPPED



SECTION C-C

PROJECT NO. 17BP.7.R.102
ROCKINGHAM COUNTY
STATION: 13+77.50 -L-

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

RIP RAP DETAILS

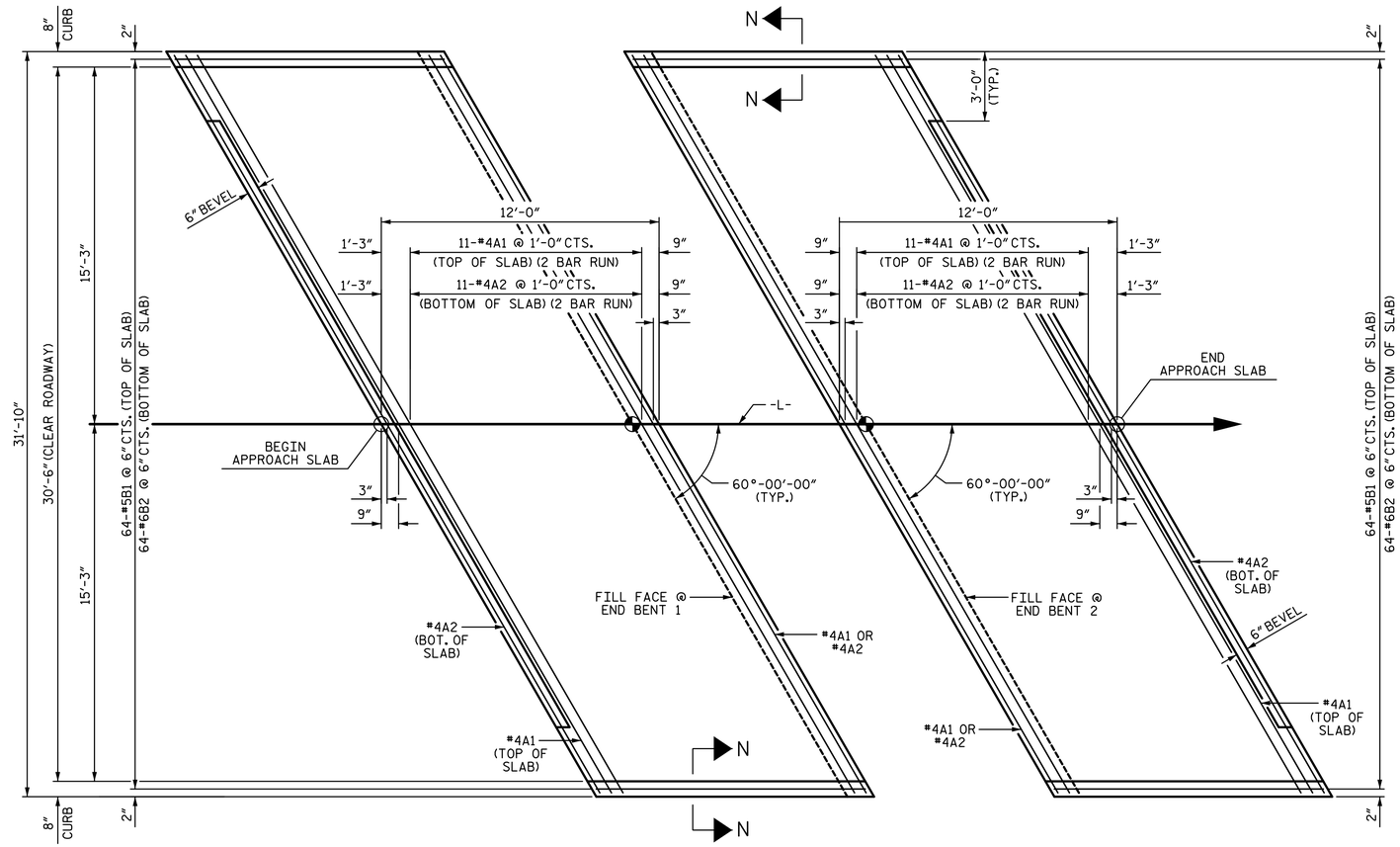
PLANS PREPARED BY:
SIMPSON ENGINEERS & ASSOCIATES
5640 Dillard Drive
Suite 200
Cary, NC 27518
(919) 852-0468
(919) 852-0598 (Fax)
www.simpsonengr.com
LICENSURE NO. C-2521

NORTH CAROLINA PROFESSIONAL SEAL 031268 ENGINEER BETSY S. COX
11/1/2016

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

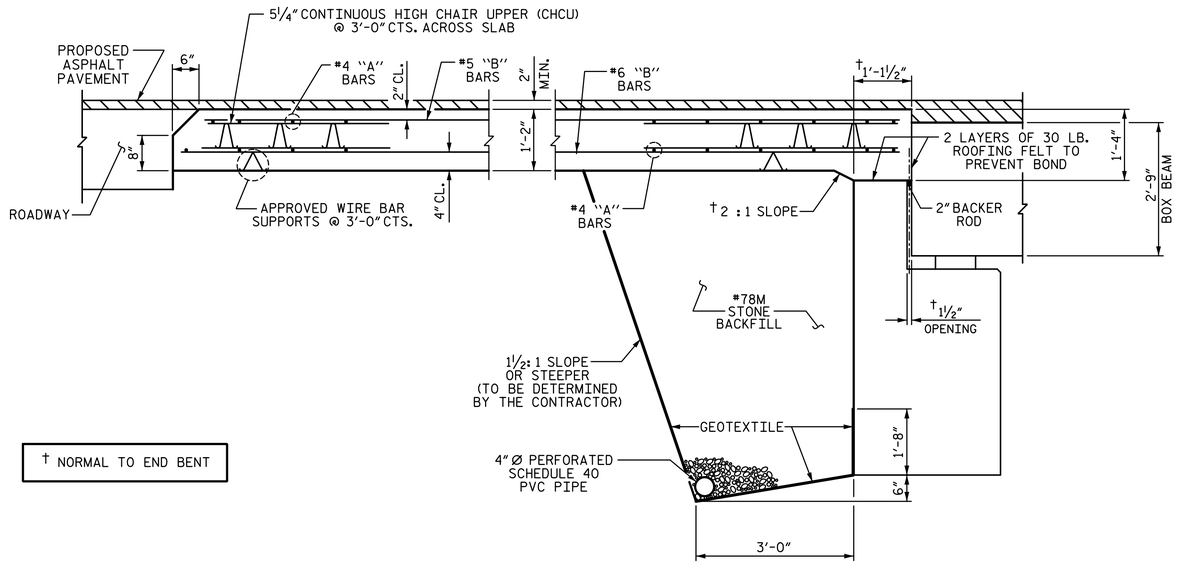
REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-17
1			3			TOTAL SHEETS
2			4			19

DRAWN BY: S.D. COOPER DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16



PLAN @ END BENT 1 PLAN @ END BENT 2

DIMENSIONS SHOWN ARE TYPICAL FOR BOTH APPROACH SLABS



† NORMAL TO END BENT

SECTION THRU SLAB

NOTES:

FOR BRIDGE APPROACH FILL INCLUDING GEOTEXTILE, 4" Ø DRAINAGE PIPE, AND #78M STONE BACKFILL, SEE ROADWAY PLANS.

GEOTEXTILE SHALL BE TYPE 1 IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 1056.

#78M STONE BACKFILL (CLASS V SELECT MATERIAL) SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS SECTION 1016.

#78M STONE BACKFILL IS TO BE CONTINUOUS ALONG FILL FACE OF BACKWALL FROM OUTSIDE EDGE TO OUTSIDE EDGE OF APPROACH SLAB.

FOR THE 4" Ø DRAINAGE PIPE OUTLET(S), SEE ROADWAY STANDARD DRAWINGS.

AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE AND SHALL BE PAVED. SEE ROADWAY PLANS.

APPROACH SLAB GROOVING IS NOT REQUIRED.

BILL OF MATERIAL

APPROACH SLAB AT EB 1

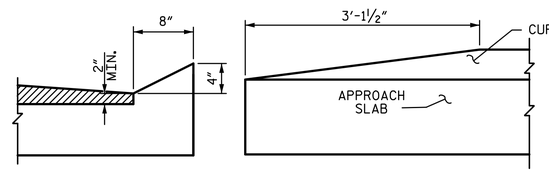
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
✳ A1	26	#4	STR	19'-3"	334
A2	26	#4	STR	19'-1"	331
✳ B1	64	#5	STR	11'-1"	740
B2	64	#6	STR	11'-7"	1113
REINFORCING STEEL				LB	1444
✳ EPOXY COATED REINFORCING STEEL				LB	1074
CLASS AA CONCRETE				CY	16.8

APPROACH SLAB AT EB 2

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
✳ A1	26	#4	STR	19'-3"	334
A2	26	#4	STR	19'-1"	331
✳ B1	64	#5	STR	11'-1"	740
B2	64	#6	STR	11'-7"	1113
REINFORCING STEEL				LB	1444
✳ EPOXY COATED REINFORCING STEEL				LB	1074
CLASS AA CONCRETE				CY	16.8

SPLICE LENGTHS

BAR SIZE	EPOXY COATED	UNCOATED
#4	2'-0"	1'-9"
#5	2'-6"	2'-2"
#6	3'-10"	2'-7"



SECTION N-N

END OF CURB WITHOUT SHOULDER BERM GUTTER

CURB DETAILS

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 1 OF 2

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

BRIDGE APPROACH SLAB FOR PRESTRESSED CONCRETE BOX BEAM UNIT (SUB-REGIONAL TIER) -60°SKEW

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-18
1			3			TOTAL SHEETS
2			4			19

DRAWN BY: S.D. COOPER DATE: 8-16
CHECKED BY: B.S. COX DATE: 8-16
DESIGN ENGINEER OF RECORD: B.S. COX DATE: 8-16

PLANS PREPARED BY:
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11/1/2016

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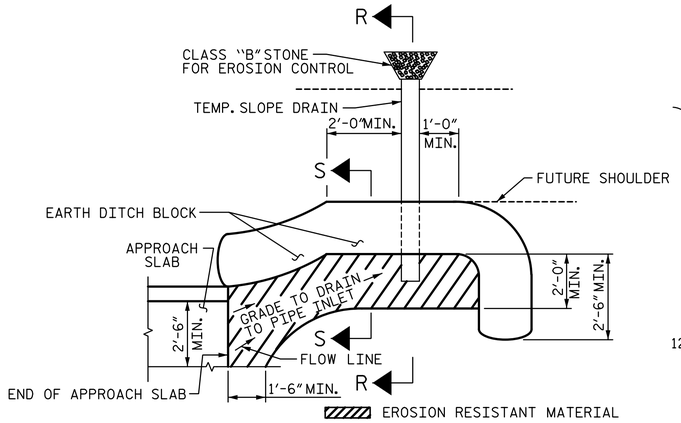
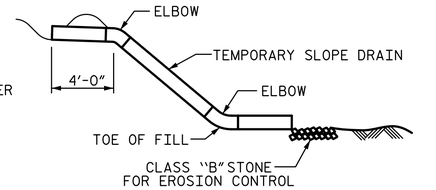
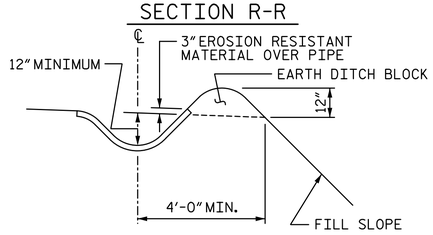


NOTE: IMMEDIATELY AFTER THE CONSTRUCTION OF THE APPROACH SLAB, THE CONTRACTOR SHALL PROVIDE TEMPORARY BERM AND SLOPE DRAIN. CONTRACTOR SHALL GRADE TO PIPE INLET AND PROVIDE EROSION RESISTANT MATERIAL AS SHOWN. THE EROSION RESISTANT MATERIAL SHALL BE EITHER 1) ASPHALT PLANT MIX, TYPE 1 OR TYPE 2, MIN. 2" DEPTH, 2) EROSION CONTROL MAT, OR 3) CONCRETE, AS DIRECTED BY THE ENGINEER. THE SLOPE DRAIN SHALL CONSIST OF A NON-PERFORATED TEMPORARY DRAINAGE PIPE, 12 INCHES IN DIAMETER.

PLAN VIEW



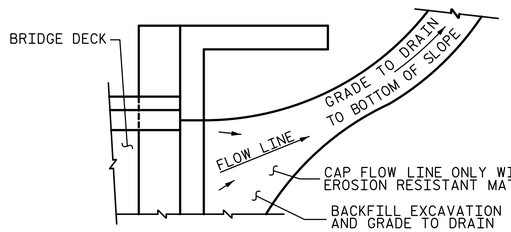
SECTION R-R



SECTION S-S

TEMPORARY BERM AND SLOPE DRAIN DETAILS

(TO BE USED WHEN SHOULDER BERM GUTTER IS REQUIRED)



NOTE: IF THE APPROACH SLAB IS NOT CONSTRUCTED IMMEDIATELY AFTER THE BACKFILLING OF THE END BENT EXCAVATION, GRADE TO DRAIN TO THE BOTTOM OF THE SLOPE AND PROVIDE EROSION RESISTANT MATERIAL, SUCH AS FIBERGLASS ROVING OR AS DIRECTED BY THE ENGINEER TO PREVENT SOIL EROSION AND TO PROTECT THE AREA ADJACENT TO THE STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO REMOVE THESE MATERIALS PRIOR TO CONSTRUCTION OF THE APPROACH SLAB.

TEMPORARY DRAINAGE DETAIL

PROJECT NO. 17BP.7.R.102

ROCKINGHAM COUNTY

STATION: 13+77.50 -L-

SHEET 2 OF 2

DRAWN BY: S.D. COOPER	DATE: 8-16		
CHECKED BY: B.S. COX	DATE: 8-16		
DESIGN ENGINEER OF RECORD: B.S. COX	DATE: 8-16		

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11/1/2016

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STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

BRIDGE APPROACH SLAB DETAILS

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-19
1			3			TOTAL SHEETS
2			4			19

STANDARD NOTES

DESIGN DATA:

SPECIFICATIONS	A.A.S.H.T.O. (CURRENT)
LIVE LOAD	SEE PLANS
IMPACT ALLOWANCE	SEE A.A.S.H.T.O.
STRESS IN EXTREME FIBER OF	
STRUCTURAL STEEL - AASHTO M270 GRADE 36	20,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50W	27,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50	27,000 LBS. PER SQ. IN.
REINFORCING STEEL IN TENSION	
GRADE 60	24,000 LBS. PER SQ. IN.
CONCRETE IN COMPRESSION	1,200 LBS. PER SQ. IN.
CONCRETE IN SHEAR	SEE A.A.S.H.T.O.
STRUCTURAL TIMBER - TREATED OR	
UNTREATED - EXTREME FIBER STRESS	1,800 LBS. PER SQ. IN.
COMPRESSION PERPENDICULAR TO GRAIN OF TIMBER	375 LBS. PER SQ. IN.
EQUIVALENT FLUID PRESSURE OF EARTH	30 LBS. PER CU. FT. (MINIMUM)

MATERIAL AND WORKMANSHIP:

EXCEPT AS MAY OTHERWISE BE SPECIFIED ON PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE 2012 "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" OF THE N. C. DEPARTMENT OF TRANSPORTATION.

STEEL SHEET PILING FOR PERMANENT OR TEMPORARY APPLICATIONS SHALL BE HOT ROLLED.

CONCRETE:

UNLESS OTHERWISE REQUIRED ON PLANS, CLASS A CONCRETE SHALL BE USED FOR ALL PORTIONS OF ALL STRUCTURES WITH THE EXCEPTION THAT: CLASS AA CONCRETE SHALL BE USED IN BRIDGE SUPERSTRUCTURES, ABUTMENT BACKWALLS, AND APPROACH SLABS; AND CLASS B CONCRETE SHALL BE USED FOR SLOPE PROTECTION AND RIP RAP.

CONCRETE CHAMFERS:

UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED CORNERS ON STRUCTURES SHALL BE CHAMFERED 3/4" WITH THE FOLLOWING EXCEPTIONS: TOP CORNERS OF CURBS MAY BE ROUNDED TO 1-1/2" RADIUS WHICH IS BUILT INTO CURB FORMS; CORNERS OF TRANSVERSE FLOOR EXPANSION JOINTS SHALL BE ROUNDED WITH A 1/4" FINISHING TOOL UNLESS OTHERWISE REQUIRED ON PLANS; AND CORNERS OF EXPANSION JOINTS IN THE ROADWAY FACES AND TOPS OF CURBS AND SIDEWALKS SHALL BE ROUNDED TO A 1/4" RADIUS WITH A FINISHING STONE OR TOOL UNLESS OTHERWISE REQUIRED ON PLANS.

DOWELS:

DOWELS WHEN INDICATED ON PLANS AS FOR CULVERT EXTENSIONS, SHALL BE EMBEDDED AT LEAST 12" INTO THE OLD CONCRETE AND GROUTED INTO PLACE WITH 1:2 CEMENT MORTAR.

ALLOWANCE FOR DEAD LOAD DEFLECTION, SETTLEMENT: ETC. IN CASTING SUPERSTRUCTURES:

BRIDGES SHALL BE BUILT ON THE GRADE OR VERTICAL CURVE SHOWN ON PLANS. SLABS, CURBS AND PARAPETS SHALL CONFORM TO THE GRADE OR CURVE.

ALL DIMENSIONS WHICH ARE GIVEN IN SECTION AND ARE AFFECTED BY DEAD LOAD DEFLECTIONS ARE DIMENSIONS AT CENTER LINE OF BEARING UNLESS OTHERWISE NOTED ON PLANS. IN SETTING FORMS FOR STEEL BEAM BRIDGES AND PRESTRESSED CONCRETE GIRDER BRIDGES, ADJUSTMENTS SHALL BE MADE DUE TO THE DEAD LOAD DEFLECTIONS FOR THE ELEVATIONS SHOWN. WHERE BLOCKS ARE SHOWN OVER BEAMS FOR BUILDING UP TO THE SLAB, THE VERTICAL DIMENSIONS OF THE BLOCKS SHALL BE ADJUSTED BETWEEN BEARINGS TO COMPENSATE FOR DEAD LOAD DEFLECTIONS, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER. WHERE BOTTOM OF SLAB IS IN LINE WITH BOTTOM OF TOP FLANGES, DEPTH OF SLAB BETWEEN BEARINGS SHALL BE ADJUSTED TO COMPENSATE FOR DEAD LOAD DEFLECTION, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER.

IN SETTING FALSEWORK AND FORMS FOR REINFORCED CONCRETE SPANS, AN ALLOWANCE SHALL BE MADE FOR DEAD LOAD DEFLECTIONS, SETTLEMENT OF FALSEWORK, AND PERMANENT CAMBER WHICH SHALL BE PROVIDED FOR IN ADDITION TO THE ELEVATIONS SHOWN. AFTER REMOVAL OF THE FALSEWORK, THE FINISHED STRUCTURES SHALL CONFORM TO THE PROFILE AND ELEVATIONS SHOWN ON THE PLANS AND CONSTRUCTION ELEVATIONS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR FALSEWORK OR FORMS FOR BRIDGE SUPERSTRUCTURE AND ANY STRUCTURE OR PARTS OF A STRUCTURE AS NOTED ON THE PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE CONSTRUCTION OF THE FALSEWORK OR FORMS IS STARTED.

REINFORCING STEEL:

ALL REINFORCING STEEL SHALL BE DEFORMED. DIMENSIONS RELATIVE TO PLACEMENT OF REINFORCING ARE TO CENTERS OF BARS UNLESS OTHERWISE INDICATED IN THE PLANS. DIMENSIONS ON BAR DETAILS ARE TO CENTERS OF BARS OR ARE OUT TO OUT AS INDICATED ON PLANS.

WIRE BAR SUPPORTS SHALL BE PROVIDED FOR REINFORCING STEEL WHERE INDICATED ON THE PLANS. WHEN BAR SUPPORT PIECES ARE PLACED IN CONTINUOUS LINES, THEY SHALL BE SO PLACED THAT THE ENDS OF THE SUPPORTING WIRES SHALL BE LAPPED TO LOCK LEGS ON ADJOINING PIECES.

STRUCTURAL STEEL:

AT THE CONTRACTOR'S OPTION, HE MAY SUBSTITUTE 7/8" Ø SHEAR STUDS FOR THE 3/4" Ø STUDS SPECIFIED ON THE PLANS. THIS SUBSTITUTION SHALL BE MADE AT THE RATE OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS, AND STUD SPACING CHANGES SHALL BE MADE AS NECESSARY TO PROVIDE THE SAME EQUIVALENT NUMBER OF 7/8" Ø STUDS ALONG THE BEAM AS SHOWN FOR 3/4" Ø STUDS BASED ON THE RATIO OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS. STUDS OF THE LENGTH SPECIFIED ON THE PLANS MUST BE PROVIDED. THE MAXIMUM SPACING SHALL BE 2'-0".

EXCEPT AT THE INTERIOR SUPPORTS OF CONTINUOUS BEAMS WHERE THE COVER PLATE IS IN CONTACT WITH BEARING PLATE, THE CONTRACTOR MAY, AT HIS OPTION, SUBSTITUTE FOR THE COVER PLATES DESIGNATED ON THE PLANS COVER PLATES OF THE EQUIVALENT AREA PROVIDED THESE PLATES ARE AT LEAST 5/16" IN THICKNESS AND DO NOT EXCEED A WIDTH EQUAL TO THE FLANGE WIDTH LESS 2" OR A THICKNESS EQUAL TO 2 TIMES THE FLANGE THICKNESS. THE SIZE OF FILLET WELDS SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT ANSI/AASHTO/AWS "BRIDGE WELDING CODE". ELECTROSLAG WELDING WILL NOT BE PERMITTED.

WITH THE SOLE EXCEPTION OF EDGES AT SURFACES WHICH BEAR ON OTHER SURFACES, ALL SHARP EDGES AND ENDS OF SHAPES AND PLATES SHALL BE SLIGHTLY ROUNDED BY SUITABLE MEANS TO A RADIUS OF APPROXIMATELY 1/16 INCH OR EQUIVALENT FLAT SURFACE AT A SUITABLE ANGLE PRIOR TO PAINTING, GALVANIZING, OR METALLIZING.

HANDRAILS AND POSTS:

METAL STANDARDS AND FACES OF THE CONCRETE END POSTS FOR THE METAL RAIL SHALL BE SET NORMAL TO THE GRADE OF THE CURB, UNLESS OTHERWISE SHOWN ON PLANS. THE METAL RAIL AND TOPS OF CONCRETE POSTS USED WITH THE ALUMINUM RAIL SHALL BE BUILT PARALLEL TO THE GRADE OF THE CURB.

METAL HANDRAILS SHALL BE IN ACCORDANCE WITH THE PLANS. RAILS SHALL BE AS MANUFACTURED FOR BRIDGE RAILING. CASTINGS SHALL BE OF A UNIFORM APPEARANCE. FINS AND OTHER DEFORMATIONS RESULTING FROM CASTING OR OTHERWISE SHALL BE REMOVED IN A MANNER SO THAT A UNIFORM COLORING OF THE COMPLETED CASTING SHALL BE OBTAINED. CASTINGS WITH DISCOLORATIONS OR OF NON-UNIFORM COLORING WILL NOT BE ACCEPTED. CERTIFIED MILL REPORTS ARE REQUIRED FOR METAL RAILS AND POSTS.

SPECIAL NOTES:

GENERALLY, IN CASE OF DISCREPANCY, THIS STANDARD SHEET OF NOTES SHALL GOVERN OVER THE SPECIFICATIONS, BUT THE REMAINDER OF THE PLANS SHALL GOVERN OVER NOTES HEREON, AND SPECIAL PROVISIONS SHALL GOVERN OVER ALL. SEE SPECIFICATIONS ARTICLE 105-4.

ENGLISH

JANUARY, 1990